



PARKING ADVISORY COMMITTEE AGENDA
Wednesday, August 20, 2025 - 6:00 PM
Council Chambers, 169 SW Coast Hwy, Newport, Oregon 97365

All public meetings of the City of Newport will be held in the City Council Chambers of the Newport City Hall, 169 SW Coast Highway, Newport. The meeting location is accessible to persons with disabilities. A request for an interpreter, or for other accommodations, should be made at least 48 hours in advance of the meeting to the City Recorder at 541.574.0613, or cityrecorder@newportoregon.gov.

All meetings are live-streamed at <https://newportoregon.gov>, and broadcast on Charter Channel 190. Anyone wishing to provide written public comment should send the comment to publiccomment@newportoregon.gov. Public comment must be received four hours prior to a scheduled meeting. For example, if a meeting is to be held at 3:00 P.M., the deadline to submit written comment is 11:00 A.M. If a meeting is scheduled to occur before noon, the written comment must be submitted by 5:00 P.M. the previous day. To provide virtual public comment during a city meeting, a request must be made to the meeting staff at least 24 hours prior to the start of the meeting. This provision applies only to public comment and presenters outside the area and/or unable to physically attend an in person meeting.

The agenda may be amended during the meeting to add or delete items, change the order of agenda items, or discuss any other business deemed necessary at the time of the meeting.

1. WELCOME AND INTRODUCTIONS

2. ROLL CALL

3. APPROVAL OF MINUTES

- 3.1 June 18, 2025 Parking Advisory Committee Meeting.**
[Draft Parking Advisory Comm Mtg Minutes 06-18-2025](#)
[06-18-2024 Parking Advisory Committee Meeting Video Link](#)

4. DISCUSSION ITEMS

- 4.1 Review Preliminary Plans for the Bayfront Sidewalk Improvement Project.**
- 4.2 Discussion of Nye Beach Parking Changes for October Outreach.**

4.3 Discussion of Nye Beach Parking Changes for October Outreach.

4.4 Parking Enforcement Issues.

4.5 Public Arts Committee Proposal for Sculptures on the Bayfront.

5. PUBLIC COMMENT

This is an opportunity for members of the audience to bring to the Work Group's attention any item not listed on the agenda. Comments will be limited to three (3) minutes per person with a maximum of 15 minutes for all items. Speakers may not yield their time to others.

6. ADJOURNMENT

HANDOUTS

Meeting Materials:

[Memorandum](#)

[Draft Bayfront Sidewalk Improvement Plans](#)

[City Center Parking District Boundary Options Map](#)

[Urbsworks Form Based Code Presentation](#)

[Urbsworks Form Based Code Concepts Part 1](#)

[Urbsworks Form Based Code Concepts Part 2](#)

[Draft Resolution - Nye Beach Parking Changes](#)

[Nye Beach Parking Presentation - 4.7.25 Council Work Session](#)

[Draft Nye Beach Parking FAQ](#)

[Email from Councilor Emond Regarding Sculptures on the Bayfront](#)

[Car Boot Article](#)

[Day Use Fees for South Beach State Park and South Jetty](#)

**City of Newport
Draft Parking Advisory Committee Minutes
June 18, 2025**

LOCATION: CITY COUNCIL CHAMBERS, NEWPORT CITY HALL, 169 SW COAST HIGHWAY, NEWPORT	
Time Start: 6:05 P.M.	Time End: 8:02 P.M.

ATTENDANCE LOG/ROLLCALL

COMMITTEE MEMBER	STAFF
Chair Janell Goplen (absent, excused)	Derrick Tokos, Community Development Director
Aaron Bretz (absent)	Sherri Marineau, Community Development Dept.
Gary Ripka (absent)	Coren Loper, Parking Enforcement Officer
Bill Branigan	John Fuller, Communications Specialist
Doretta Smith (by video)	
Lisa Emond (absent, excused)	PUBLIC
Kevin Lewis	Ashley Bixler
Robert Emond	
Jocelyn Stody	
Doris Posner	

AGENDA ITEM	ACTIONS
CALL TO ORDER AND ROLL CALL a. Roll Call	None.
APPROVAL OF THE MINUTES a. Meeting minutes of May 21, 2025	Motion by Lewis, seconded by Stody to approve the minutes of May 21, 2025 as written. Motion carried unanimously with Branigan, Smith, Lewis, Stody, and Posner all voting in favor.
RESULTS OF THE BAYFRONT PARKING SURVEY.	<p>Fuller gave an overview of the results of the Bayfront Parking Survey.</p> <p>The Committee discussed several aspects related to Bayfront parking, beginning with the number of businesses in the area and how many driveway aprons could potentially be removed to create additional parking spaces.</p> <p>Concerns were raised about the possibility of QR Code scams, followed by a suggestion that the city provide QR Code stickers and payment instructions for businesses on the Bayfront. There was interest in exploring a park-and-ride option along the Bayfront, starting near the NW Natural LNG tank.</p>

	<p>Committee members questioned whether it would be feasible to paint lines on sidewalks to designate parking areas, and proposed adding larger format signs in parking lots as well as distributing informational fliers in local businesses. A recommendation was made to include directional arrows on kiosks to improve usability, and a request was voiced to share parking information with short-term rentals and hotels.</p> <p>The Committee also discussed adding a parking map link to the Discover Newport website, and reviewed height requirements for parking signage. They concluded with next steps to address survey responses, and considered methods to track parking space occupancy across different times and days.</p>
SCOPE OF IMPROVEMENTS FOR THE BAYFRONT SIDEWALK ENHANCEMENT PROJECT.	<p>Tokos updated the Committee on the scope of improvements for the Bayfront sidewalk enhancement project.</p> <p>The Committee raised several questions, including whether property owners were responsible for repairing sidewalks, and inquiries about the angled parking construction planned at the Pine Street intersection. There was also a question regarding potential improvements to the sidewalks along Canyon Way Street, and discussion on how runoff will be addressed at the Hurbert and Bay Blvd section.</p>
OUTREACH SCHEDULE FOR NYE BEACH PARKING MANAGEMENT PROGRAM.	<p>Tokos gave a brief update on the schedule for the Nye Beach Parking Management Program.</p> <p>Posner requested that the group walk the Nye Beach area during another meeting so they had a better understanding of the area when talking to the community.</p> <p>The Committee discussed the options for the Tap House parking lot, and considerations for parking at the Visual Arts Center.</p>
PARKING CODE ENFORCEMENT ISSUES.	<p>Tokos reported that he and the City Engineer met with Pacific Seafoods regarding the addition of parking spaces in the Abbey Street parking lot, which was interfering with their large truck deliveries. Pacific Seafoods requested the removal of one parking space and the establishment of a loading-only space adjacent to it, which would allow trucks to maneuver through the area more effectively.</p> <p>The Committee expressed general consensus that the change would be appropriate, provided Pacific Seafoods could demonstrate they have rights to use the Bay Market driveway.</p>
CITIZEN/PUBLIC COMMENT	

Ashley Bixler asked whether someone could receive a ticket while walking to pay for parking. Tokos responded that the Enforcement Officer, Loper should be able to recognize such situations; while it was technically possible, it didn't occur frequently. Smith added that the T2 system should be able to detect when someone paid for parking within 10 minutes of the license plate recognition scan, and Tokos suggested they look into that capability.

Bixler then asked if a warning ticket could be issued for overstaying a paid parking period. Posner emphasized the importance of personal responsibility, while Bixler proposed a smaller fee for first-time offenses. Tokos reminded the group that Loper could use discretion for this and when equipment wasn't functioning properly. Posner suggested gathering data on how many individuals had received only one ticket. Tokos confirmed that data from the Bayfront would continue to be shared with the Committee. Bixler expressed concern for individuals who struggle with technology, and Smith noted that part of the goal with the meter implementation was to encourage behavior change to improve parking availability on the Bayfront.

Bixler also asked how long food trucks could remain parked, and Tokos believed the limit was two hours. She inquired whether signage indicated that payment could be made before 11 a.m.; Tokos clarified that while it wasn't stated on the signs, the system would notify users that their paid time would begin at 11 a.m. Lastly, Bixler expressed support for having QR codes available in stores for patrons to use.


Submitted by: _____

Sherri Marineau, Executive Assistant

June 18, 2025 - Parking Advisory Committee Meeting Video Link:

https://thecityofnewport.granicus.com/player/clip/1460?view_id=44&redirect=true

Memorandum

To: Parking Advisory Committee
From: Derrick Tokos, Community Development Director 
Date: August 14, 2025
Re: Topics for August 20th Parking Advisory Committee Meeting

For this meeting, we have identified five agenda items for the Parking Advisory Committee's consideration. The first item is a review of preliminary plans for the Bayfront sidewalk improvement project (enclosed). City Engineer, Chris Beatty, will be in attendance to walk through the different components of the project. One area in particular that we want to cover with the group are options for the parking area below the Port Dock Four condominiums that is leased by the Port of Newport.

The second item is an update on a form based code that is being developed as part of the City Center Revitalization Planning effort and how it will impact the City Center Parking District. Attached is a map titled "City Center Parking District Boundary Options," which will be the principle focus of the discussion. Also enclosed, is a PowerPoint presentation by the consulting firm Urbsworks, summarizing key components of the form based code, and a full draft of the concepts. The documents from Urbsworks are being provided for context, and we do not expect you to thoroughly review the materials by Wednesday. At its August 11, 2025 work session, the Planning Commission provided direction on how they would like to see the boundary of the City Center Parking District changed to align with the form based code revisions. We will be amending the boundary description of the District to match the area identified on the map as "Proposed Traditional Downtown Zone," and I'll come prepared to explain why the change is needed.

A third agenda item will be a review of the Nye Beach parking changes discussed with stakeholders in the spring of this year to see if it is the baseline that the Parking Committee wants to advance when we initiate outreach in October. Attached is a draft City Council resolution summarizing the changes, and they are also addressed in a PowerPoint presentation to Council from last April and in a draft FAQ (enclosed).

The fourth agenda item is a placeholder for parking enforcement related issues the Committee would like to discuss. Either Enforcement Officer Coren Looper or Chief Malloy should be in attendance to field your questions, and may have updates of their own.

The last item is in regards to an email from Councilor Emond indicating that the Public Arts Committee is interested in placing sculptures along the Bayfront and would like the Parking Committee's input on appropriate locations.

Attachments:

Draft Bayfront Sidewalk Improvement Plans
City Center Parking District Boundary Options Map
Urbsworks Form Based Code Presentation
Urbsworks Form Based Code Concepts Part 1
Urbsworks Form Based Code Concepts Part 2
Draft Resolution – Nye Beach Parking Changes
Nye Beach Parking Presentation – 4.7.25 Council Work Session
Draft Nye Beach Parking FAQ
Email from Councilor Emond Regarding Sculptures on the Bayfront


DATE:8/14/25 FILE:Y:\PWK\Engineering\Projects\2025-009-B25 Bayfront Abandoned Driveway Approach and Sidewalk Improvements\DESIGN\CAD\WORKING\Bayfront Sidewalk Improvements.dwg



LOCATION MAP

SHEET INDEX	
G-1	COVER SHEET
G-2	BID SHEET
G-3	SITE MAP
G-4	SITE MAP
G-5	SITE MAP
G-6	SITE MAP
C-1	856 & 839 SW BAY BLVD
C-2	813 & 769 SW BAY BLVD
C-3	746 & 701 SW BAY BLVD
C-4	669 & 617 SW BAY BLVD
C-5	444 & 440 SW BAY BLVD
C-6	415 & 411 SW BAY BLVD
C-7	404 & 367 SW BAY BLVD
C-8	156 & 146 SW BAY BLVD
C-9	106 SW BAY BLVD
C-10	106 & 103 SW BAY BLVD
T-150 & T-152	STANDARD APPROACH FOR 5' SIDEWALK & COMMERCIAL ONE-WAY AND TWO-WAY SIDEWALK DETAIL
T-210 & T-212	STANDARD SIDEWALK DETAILS & SIDEWALK AND RAMP DETAIL WITHOUT PLANTER STRIP
T-214 & T-215	TRUNCATED DOME PLACEMENT & STANDARD VAULT DETAIL
T-301 & T-0	CURBS AND GUTTER DETAILS & REINFORCED SIDEWALK DETAILS

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Sheet No.		CITY OF NEWPORT 169 SW COAST HWY, NEWPORT, OR 97365		REV.	DATE	DESCRIPTION	BY	
<div>08/12/2025</div> <div></div>		BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS						
				Designed By	Drawn By	BMF	Created By	CB
				Project No: 2025-009-0225				
		COVER SHEET		RECORD DRAWING				
				DATE: 08/12/2025				

Sheet No.	G-2
Date	08/12/2025

[illegible]

CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS

OVERALL PROJECT SITE PLAN

G-3
08/12/2025

[illegible]

CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS

PROFILE 1

G-4

08/12/2025

REV.	DATE	DESCRIPTION	BV

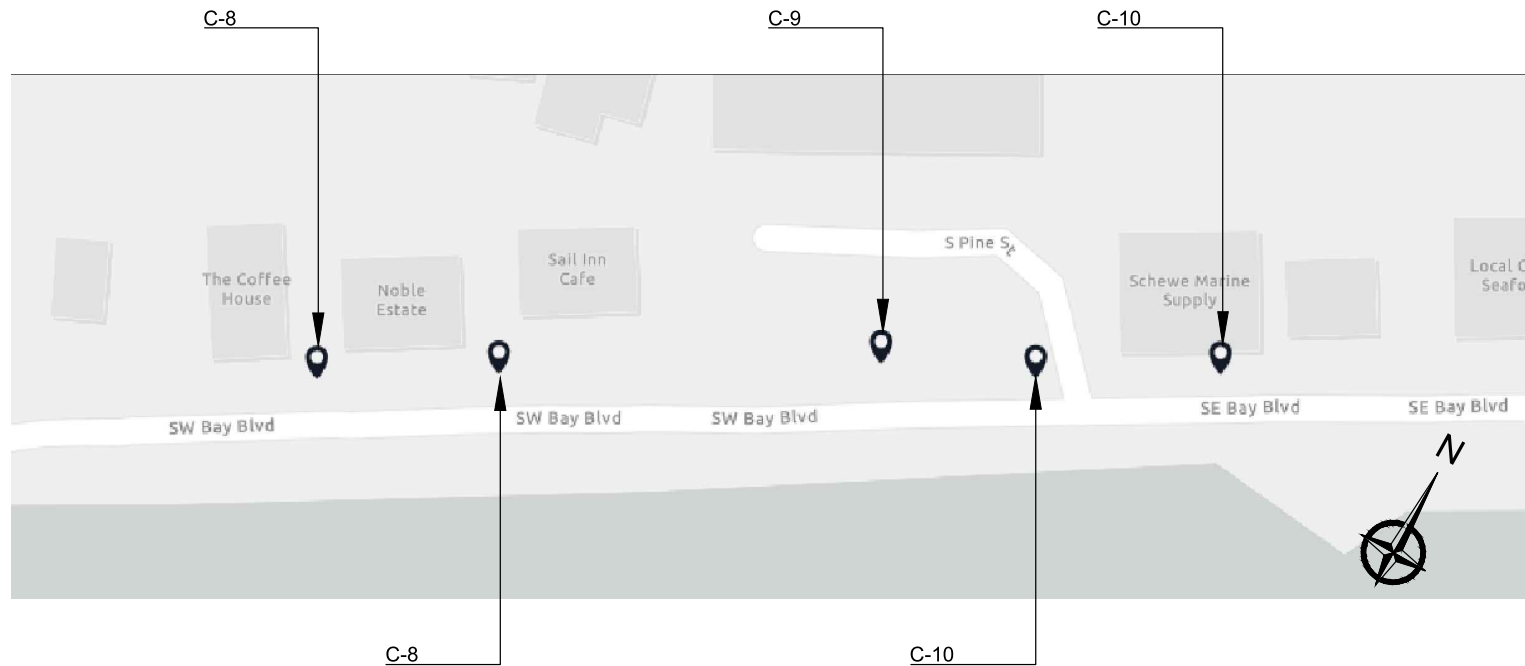
Designed By:	BM	Drawn By:	BM	Checked By:	CB
Project No:		2025-009-B2S			

RECORD DRAWING

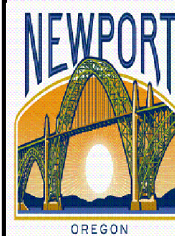
DATE: 08/12/2025

CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365
BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS
PROFILE 2

Sheet No.	G-5
Date	08/12/2025



This record drawing has been prepared, in part, based upon information furnished by others. While this information is believed to be reliable, the Engineer assumes no responsibility for the accuracy of this record drawing or for any errors or omissions that may have been incorporated into it as a result of incorrect information provided to the Engineer.



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Project No:		2025-009-625			

RECORD DRAWING

DATE: 08/12/2025

CITY OF NEWPORT

169 SW COAST HWY, NEWPORT, OR 97365

BAYFRONT ABANDONED DRIVEWAY

APPROACH AND SIDEWALK IMPROVEMENTS

PROFILE 3



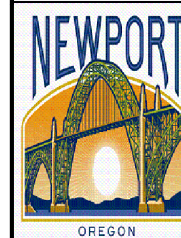
08/12/2025

A diagram of a shaded sector of a circle. The sector is defined by two radii of 6' and 11', and an arc length of 9.9'. A circular hole with a diameter of 5.6' is located within the sector. The entire area of the sector, excluding the hole, is shaded with diagonal lines.

A photograph of a waterfront area. In the foreground, there is a paved sidewalk with several rectangular utility covers. A black metal signpost stands on the sidewalk, holding a white rectangular sign with a red circle and a diagonal line, indicating 'No Left Turn'. To the left of the signpost is a chain-link fence. Behind the fence is a wooden boardwalk made of weathered planks. In the background, a large, multi-story blue building with many windows is visible. To the right of the building, there is a body of water and a distant pier or bridge structure. The sky is clear and blue.

- NOTES:**
1. PROTECT CURB DURING CONSTRUCTION
 3. PROTECT HYDRANT DURING CONSTRUCTION
 4. PROTECT FENCE DURING CONSTRUCTION
 5. CHECK FENCE OWNERSHIP MOVED BACK OUT OF ROW
 6. CONTRACTOR TO MEASURE ALL CUT OUTS
 7. PREPARE 55 SF GRAVEL AREA; INSTALL CONCRETE SIDEWALK; SEE STANDARD DRAWING T-210 FOR CONSTRUCTION DETAILS
 8. PREPARE 4.8' X 2.9' AREA; INFILL SIDEWALK; SEE STANDARD DRAWING T-210 AND T-0 FOR CONSTRUCTION DETAILS

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<div> <div>CITY OF NEWPORT</div> <div>169 SW COUNTRY HWY, NEWPORT, OR 97365</div> </div>	<div> <div>REV.</div> <div>DATE</div> </div>	<div> <div>DESCRIPTION</div> </div>	<div> <div>BY</div> </div>
<div> <div>BAYFRONT ABANDONED DRIVEWAY</div> <div>APPROACH AND SIDEWALK IMPROVEMENTS</div> </div>	<div> <div>Designed By:</div> <div></div> </div>	<div> <div>Drawn By:</div> <div>BML</div> </div>	<div> <div>Checked By:</div> <div>C8</div> </div>
	<div> <div>Project No:</div> <div>2025-009-B25</div> </div>		
<div> <div>866 AND 839 SW BAY BLVD</div> </div>	<div> <div>RECORD DRAWING</div> <div>DATE: 08/12/2025</div> </div>		

DATE: 8/14/25 FILE: Y:\PWK\Engineering\Projects\2025-009-B25 Bayfront Abandoned Driveway Approach and Sidewalk Improvements\DESIGN\CAD\WORKING\Bayfront Sidewalk Improvements.dwg

- 813 SW BAY BLVD



8

NEWPORT

OREGON

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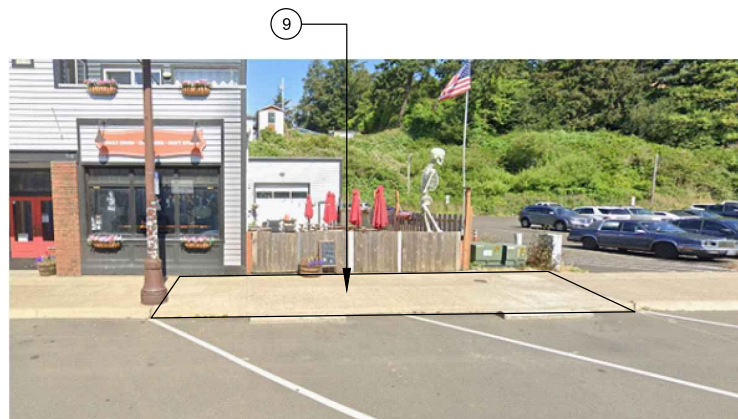
CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

**BAYFRONT ABANDONED DRIVEWAY
APPROACH AND SIDEWALK IMPROVEMENT**

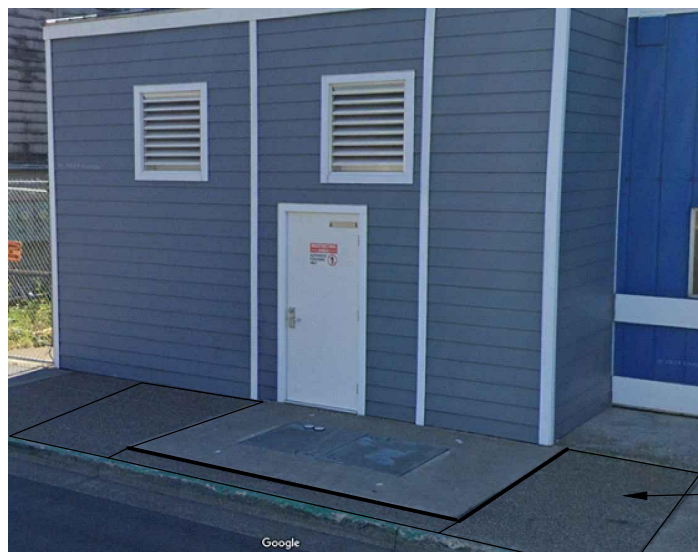
813 AND 769 SW BAY BLVD

C-2

08/12/2025



746 SW BAY BLVD

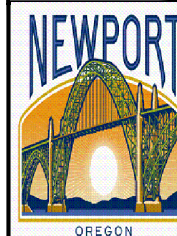


701 SW BAY BLVD

NOTES:

1. INSTALL TYPE C CURB. MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
2. INSTALL CURB AND GUTTER TO MATCH TOP OF VAULT ELEVATION; SLOPE TOP OF CURB TO MATCH ELEVATIONS AT TIE IN POINTS
3. PROTECT PRIVATE STRUCTURES DURING CONSTRUCTION
4. PROTECT VAULT DURING CONSTRUCTION
5. CONCRETE SPLASH OF PRIVATE PROPERTY PROHIBITED.
6. PROTECT STREET LIGHT DURING CONSTRUCTION. LIFT LIGHT SKIRT BEFORE POUR.
7. CONTRACTOR TO MEASURE ALL CUTOUTS
8. PREPARE 19.5' X 8' AREA; INSTALL SIDEWALK FLUSH WITH TOP ELEVATION OF VAULT; SLOPE TO MATCH EXISTING SIDEWALKS; SEE STANDARD DRAWING T-210 AND T-0 FOR CONSTRUCTION DETAILS
9. PREPARE 33.3' X 9.5' AREA; REMOVE DRIVEWAY; SEE STANDARD DRAWING T-210 FOR CONSTRUCTION DETAILS

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Designed By:	BM	Drawn By:	BM	Checked By:	CB
Project No: 2025-009-EZS					

RECORD DRAWING

DATE: 08/12/2025

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RECORD DRAWING

DATE: 08/12/2025

CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

169 SW COAST HWY, NEWPORT, OR 97365

**BAYFRONT ABANDONED DRIVEWAY
APPROACH AND SIDEWALK IMPROVEMENT**

746 AND 701 SW BAY BLVD

C-3

08/12/2025

415 SW BAY BLVD



(6)

(7)

411 SW BAY BLVD



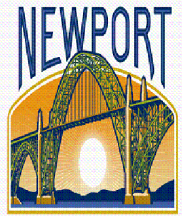
8

9


NOTES:

1. INSTALL CURB AND GUTTER. MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
2. PROTECT PRIVATE STRUCTURES DURING CONSTRUCTION
3. CONCRETE SPLASH OF PRIVATE PROPERTY PROHIBITED.
4. CHECK FENCE OWNERSHIP TO MOVE BACK OUT OF ROW
5. CONTRACTOR TO MEASURE ALL CUTOUTS
6. PREPARE 7'6" X 6.8' AREA; INSTALL SIDEWALK FLUSH WITH TOP ELEVATION OF VAULT; SEE STANDARD DRAWING T-210 AND T-152 FOR CONSTRUCTION DETAILS
7. PREPARE 6.7' X 6.3 AREA; INSTALL SIDEWALK FLUSH WITH TOP ELEVATION VAULT. SEE STANDARD DRAWING T-210 AND T-152 FOR CONSTRUCTION DETAILS
8. PREPARE 21.8' X 8.5' AREA; REPLACE SIDEWALK PANEL; SEE STANDARD DRAWING T-210 AND T-0 FOR CONSTRUCTION DETAILS
9. PREPARE 24.8' X 7.5 AREA; REMOVE DRIVEWAY; SEE STANDARD DRAWING T-210, T-150, AND T-152 FOR CONSTRUCTION DETAILS
10. ON NOTE 7, THE BACK OF WING 1 IN HIGHER THAN FRONT OF WING AT JOINT OF THROAT
11. ON NOTE 8, THE CURB IS SEPARATED FROM THE PANELS OF THE SIDEWALK FOR THE ENTIRETY OF THE SIDEWALK UNTIL IT REACHES THE ADA RAMP AT 367 SW BAY BLVD
12. OLD IMAGE OF 415 SW BAY BLVD; ONLY PARTIALLY USED ON RIGHT SIDE OF IMAGE AS OF TODAY; WILL UPDATE PLANS WITH NEW PHOTOS AND MEASUREMENTS

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	Date	08/12/2025	
	Sheet No.		
	<p>CITY OF NEWPORT 169 SW COAST HWY, NEWPORT, OR 97365</p>		
	<p>BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS</p>		
	<p>415 AND 411 SW BAY BLVD</p>		
REV.	DATE	DESCRIPTION	BY
Designed By		Drawn By	Checked By
Project No:		2025-009-825	
		B/M	GB
<p>RECORD DRAWING</p>			
DATE: 08/12/2025			

8

9

1. INSTALL TYPE C CURB; MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
2. INSTALL CURB AND GUTTER; MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
3. PROTECT PRIVATE STRUCTURES DURING CONSTRUCTION
4. PROTECT FIRE HYDRANT DURING CONSTRUCTION
5. PROTECT TELEPHONE POLE DURING CONSTRUCTION
6. CONCRETE SPLASH OF PRIVATE PROPERTY PROHIBITED.
7. CONTRACTOR TO MEASURE ALL CUTOUTS
8. PREPARE 12.3' X 7.9' AREA; REMOVE ADA RAMP; SEE STANDARD DRAWINGS T-210 AND T-212 FOR CONSTRUCTION DETAILS
9. PREPARE 12.5' X 8' AREA; REMOVE ADA RAMP; SEE STANDARD DRAWING T-210 AND T-212 FOR CONSTRUCTION DETAILS

REV.	DATE	DESCRIPTION	BY

Designed By:	BM	Drawn By:	BM	Checked By:	CB
Project No: 2025-009-525					

RECORD DRAWING

DATE: 08/12/2025

CITY OF NEWPORT
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BAYFRONT ABANDONED DRIVEWAY
APPROACH AND SIDEWALK IMPROVEMENT

404 AND 367 SW BAY BLVD

Sheet No.	C-7
Date	08/12/2025

156 SW BAY BLVD

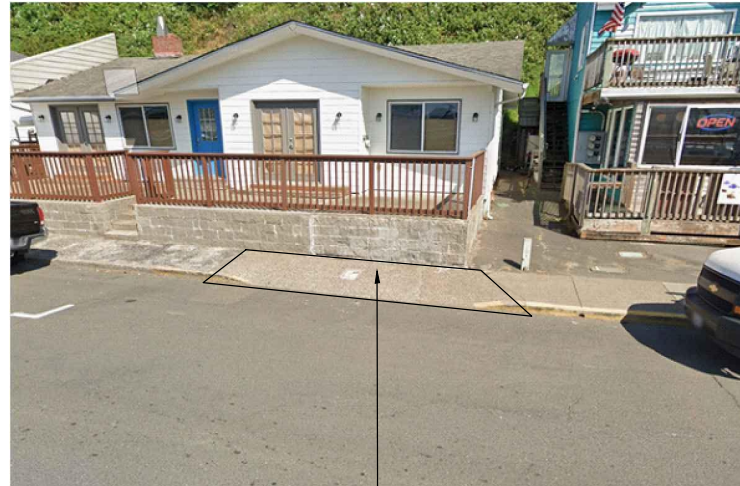


NOTES:

1. INSTALL TYPE C CURB. MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
2. PROTECT PRIVATE STRUCTURES DURING CONSTRUCTION
3. CONCRETE SPLASH OF PRIVATE PROPERTY PROHIBITED.
4. CONTRACTOR TO MEASURE ALL CUTOUTS

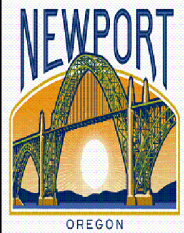
PREPARE 23.4' x 5' AREA;
- REMOVE DRIVEWAY;
SEE STANDARD DRAWING T-210
FOR CONSTRUCTION DETAILS

146 SW BAY BLVD



PREPARE 21.7' x 5' AREA;
— REMOVE DRIVEWAY;
SEE STANDARD DRAWING T-210
FOR CONSTRUCTION DETAILS

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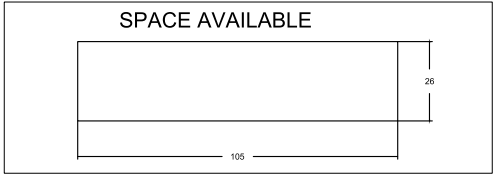
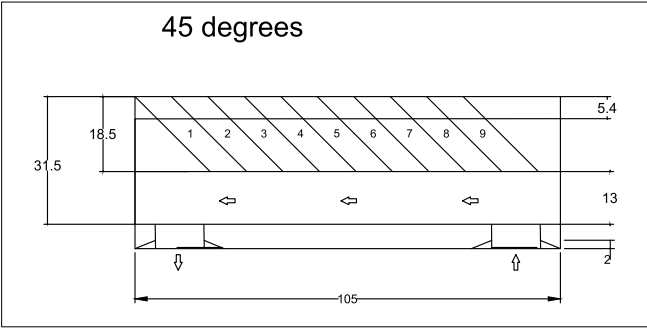
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CITY OF NEWPORT 169 SW COAST HWY, NEWPORT, OR 97365	BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS	156 AND 146 SW BAY BLVD
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Sheet No.	C-8
Date	08/12/2025

DATE: 8/14/25 FILE: P:\PWE\Engineering\Projects\2025-009-B25 Bayfront Abandoned Driveway Approach and Sidewalk Improvements\DESIGN\CAD\WORKING\Bayfront Sidewalk Improvements.dwg

PARKING ANGLE °	CURB LENGTH	STALL DEPTH		AISLE WIDTH		BAY WIDTH		STRIPE LENGTH
		SINGLE	DOUBLE	ONE	TWO	ONE	TWO	
		D1	D2	WAY A1	WAY A2	WAY B1	WAY B2	
90°	8'-6"	18'	36'	23'	23'	59'	59'	18'
60°	10'	20'	40'	17'	18'	57'	58'	23'
45°	12'	18'-6"	37'	13'	18'	50'	53'	28'-6"
30°	12'	16'-6"	33'	12'	18'	45'	51'	32'-8"
0°	22'	8'-6"	17'	12'	18'	29'	35'	8'-6"



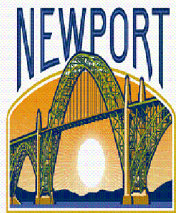
PREPARE 22' x 6' AREA;
REMOVE DRIVEWAY;
SEE STANDARD DRAWING
T-210, T-150, AND T-152
FOR CONSTRUCTION DETAILS

PREPARE 64.8' x 6' AREA;
REMOVE DRIVEWAY;
SEE STANDARD DRAWING T-210
FOR CONSTRUCTION DETAILS

PREPARE 22' x 6' AREA;
REMOVE DRIVEWAY;
SEE STANDARD DRAWING
T-210, T-150, AND T-152
FOR CONSTRUCTION DETAILS

- NOTES:
1. INSTALL TYPE C CURB; MATCH WITH EXISTING CURB AT TIE INS
 2. CREATE ONE ENTRANCE AND ONE EXIT
 3. 12 FT THROAT AND TWO 5 FT WINGS ON EACH DRIVEWAY
 4. ONE WAY DRIVEWAY
 5. PROTECT PRIVATE STRUCTURES
 6. CONCRETE SPLASH PROFITED ON
 7. CONTRACTOR TO MEASURE ALL CUTOUTS

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REV.	DATE	DESCRIPTION	BY

Drawn By	BM	Checked By	CB
2025-009-B25			

RECORD DRAWING
DATE: 08/12/2025

CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

BAYFRONT ABANDONED DRIVEWAY
APPROACH AND SIDEWALK IMPROVEMENTS

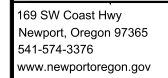
106 SW BAY BLVD

Drawn No.	08/12/2025
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1. INSTALL TYPE C CURB. MATCH EXISTING CURB ELEVATION AT TIE IN POINTS
2. PROTECT PRIVATE STRUCTURES DURING CONSTRUCTION
3. CONCRETE SPLASH OF PRIVATE PROPERTY PROHIBITED.

A photograph of a building entrance. On the left, a wooden pallet is leaning against a dark wooden wall. A white door is visible in the center. To the right, a dark-colored car is parked on a paved area. A green curb is visible in the foreground. A black arrow points upwards from the bottom center of the image.

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<div> <div>C-10</div> <div>08/12/2025</div> </div>	<div> <div>CITY OF NEWPORT</div> <div>169 SW COAST HWY, NEWPORT, OR 97365</div> </div>	<div> <div>REV.</div> <div>DATE</div> <div>DESCRIPTION</div> <div>BY</div> </div>			
<div> <div>C-10</div> <div>08/12/2025</div> </div>	<div> <div>BAYFRONT ABANDONED DRIVEWAY</div> <div>APPROACH AND SIDEWALK IMPROVEMENTS</div> </div>	<div> <div>DESIGNED BY</div> <div>BM</div> <div>DRAWN BY</div> <div>BM</div> <div>CHECKED BY</div> <div>CB</div> </div>			
		<div> <div>PROJECT NO:</div> <div>2025-009-525</div> </div>			
		<div> <div>RECORD DRAWING</div> <div>DATE: 08/12/2025</div> </div>			
		<div> <div>106 AND 103 SW BAY BLVD</div> </div>			

THERE IS A PROBLEM WITH THE RUNOFF HERE



5'

6'

CURB IS 8"

CHRIS THINKS WE WILL DO THIS
OURSELVES AS THE CONTRACTOR
WE WILL HAVE FOR THE REST OF THE
PROJECT WILL BE CONCRETE
FOCUSED AND THIS WILL REQUIRE
SPECIALIZATION WITH STORM
DRAINAGE



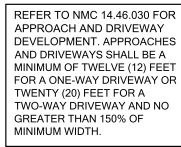
City of Newport
169 SW Coast Hwy
Newport, Oregon 97365
(541) 574-3366 Fax: (541) 265-3301

CORNER OF SE BAY BLVD AND
SE EADS STE

DETAIL NO.

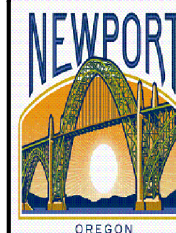
C-19

7/16/2025 24



1. A MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. SCORE MARKS ARE TO BE TOOLED INTO CONCRETE SURFACE TO FORM A SQUARE BUT NOT TO EXCEED 8" IN ANY DIRECTION.
3. REINFORCING BAR IS TO BE HELD UP WITH NON-CORROSIVE MATERIALS (ROCK, CONCRETE) PRIOR TO CONCRETE BEING PLACED.
4. REBAR SHALL BE BENT WITH GRADE BREAK TO MATCH SLOPE CHANGE WHERE WALKWAY AND RAMP MEET.
5. REBAR FRAME CAN BE USED WITH EITHER T-150 OR T-151 APPROACH LAYOUTS.

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REV.	DATE	DESCRIPTION	BY

Designed By: BM Drawn By: BM Checked By: CB

Project No: 2025-009-825

RECORD DRAWING

DATE: 08/12/2025

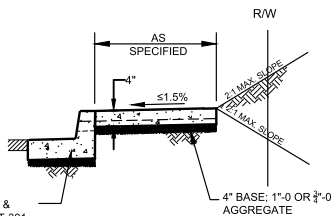
CITY OF NEWPORT
169 SW COAST HWY, NEWPORT, OR 97365

BAYFRONT ABANDONED DRIVEWAY APPROACH AND SIDEWALK IMPROVEMENTS

T-150 AND T-152

D-1

08/12/20

STANDARD CURB &
GUTTER, DETAIL T-301

NOTES:

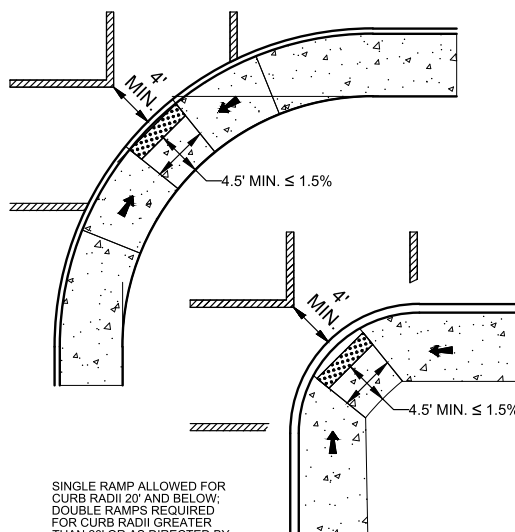
1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. STANDARD SIDEWALK CROSS SLOPE SHALL BE 1.5% TOWARD THE STREET. WHEN THE ADJACENT PROPERTY IS LOWER THAN THE TOP OF THE CURB, SIDEWALK MAY SLOPE AWAY FROM STREET.
4. DRAIN BLOCKOUTS IN THE CURB SHALL BE EXTENDED TO THE BACK OF THE SIDEWALK WITH A 3" DIA. PLASTIC PIPE AT A 1.5% SLOPE. A CONTRACTION JOINT SHALL BE PLACED OVER THE PIPE. NO COUPLINGS UNDER SIDEWALK. OUTFALL AT FACE OF CURB EQUALS GUTTER PAN ELEVATION.
5. SEE STANDARD WHEELCHAIR/BICYCLE RAMP DETAILS ODOT STANDARD DRAWING RD744, CITY STANDARD DRAWING T-212.
6. SEE STANDARD DETAILS FOR DRIVEWAYS: T-150, T-151, AND T-152.
7. WRAP PROTRUDING STRUCTURES IN SIDEWALKS WITH PREFORMED EXPANSION JOINT FILLER.



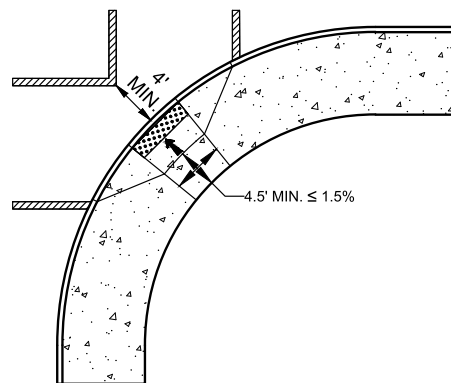
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169 SW Coast Hwy
Newport, Oregon 97365
(541) 574-3366 Fax: (541) 265-3301

STANDARD SIDEWALK DETAILS

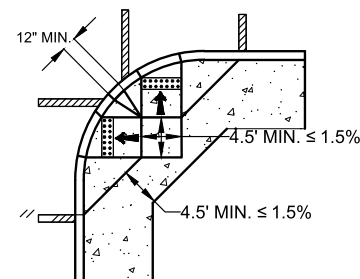
DETAIL NO.
T-210
2/12/2024



SINGLE RAMP ALLOWED FOR CURB RADII 20' AND BELOW; DOUBLE RAMPS REQUIRED FOR CURB RADII GREATER THAN 20' OR AS DIRECTED BY CITY ENGINEER.



COMBINATION RAMP (FOR WIDE SIDEWALKS;
USE IN ALTERATIONS ONLY AND WHEN SITE
CONSTRAINTS PROHIBIT INSTALLING TWO RAMP)



PERPENDICULAR RAMP (FOR
NARROW SIDEWALKS)

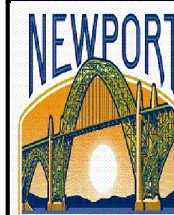
SIDEWALK AND RAMP DETAIL WITHOUT PLANTER STRIP

DETAIL NO.
T-212
2/12/2024

- GENERAL NOTES FOR ALL DETAILS:

1. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. SIDEWALK RAMP DETAILS ARE BASED ON UNITED STATES ACCESS BOARD STANDARDS.
3. ALL SIDEWALK AND RAMP GRADES SHALL MEET ADA STANDARDS.
4. TOOLED JOINTS ARE REQUIRED AT ALL SIDEWALK RAMP SLOPE BREAK LINES.
5. SIDEWALK CURB RAMP SLOPES SHOWN ARE RELATIVE TO THE TRUE LEVEL HORIZON (ZERO BUBBLE).
6. PLACE TRUNCATED DOME DETAILABLE WALKWAY SURFACE IN THROAT OF RAMP CLOSEST TO STREET. SEE STANDARD DRAWING T-213.
7. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN RAMP PATHWAY.
8. CROSSWALK LINES WHERE REQUIRED SHALL BE AS PER STRIPING AND APPLIED BY THE CITY ENGINEER.
9. SEE STANDARD SIDEWALK DETAIL T-210 FOR ADDITIONAL INFORMATION.
10. CONFORM WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, SECTION 3B.18 FOR PEDESTRIAN CROSSWALK MARKINGS.
11. REFER TO ODOT STANDARD DRAWING RD 759 & RD 759 FOR TRUNCATED DOME PLACEMENT

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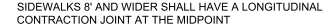
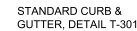
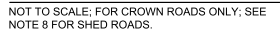


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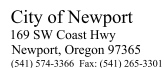
Sheet No.	CITY OF NEWPORT		REV.	DATE	DESCRIPTION	BY
1	169 SW COAST HWY, NEWPORT, OR 97365					
	BAYFRONT ABANDONED DRIVEWAY					
	APPROACH AND SIDEWALK IMPROVEMENTS					
D-2		T-210 AND T-212		RECORD DRAWING DATE: 08/12/2025		



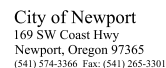


1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR.
2. CURB STRUCTURES SHALL CONFORM TO THE OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. CONTRACTION JOINTS
 - A) TO BE PROVIDED
 - AT EACH POINT OF TANGENCY
 - AT EACH SIDE OF INLET STRUCTURES
 - AT BOTH SIDES OF AN APPROACH
 - B) SPACING TO BE NOT MORE THAN 15 FEET
 - C) DEPTH OF JOINT SHALL BE AT LEAST 1/3 THICKNESS OF CONCRETE
3. EXPANSION JOINTS
 - A) TO BE PROVIDED
 - AT EACH COLD JOINT
 - AT EACH JOINT BETWEEN NEW AND EXISTING CONCRETE
 - B) EXPANSION JOINTS IN CURB & GUTTER SHALL BE PLACED AT MAX 45' INTERVALS
 - C) DEPTH OF JOINT SHALL BE EQUAL TO THICKNESS OF CONCRETE
 - D) EXPANSION JOINTS SHALL USE PREFORMED EXPANSION JOINT FILLER
4. WEEPHOLES
 - A) DRAINAGE ACCESS THROUGH EXISTING CURBS SHALL BE DONE BY:
 - CORE DRILLING, OR
 - VERTICAL SAWCUT OF CURB 18" EACH SIDE OF DRAIN AND RE-POURED (WITH DOWELS BOTH SIDES (CASTING CUT)) TO FULL DEPTH OF CURB OR CURB & GUTTER
5. STAMP TOP OF CURB WITH "W" AT WATER SERVICE CROSSING AND "S" AT SANITARY LATERAL CROSSING AS SPECIFIED.
6. SEE STANDARD CURB CUT DETAIL FOR DRIVEWAYS: T-150, T-151, AND T-152.
7. TYPE C CURB MAY BE USED FOR REPLACEMENT OF EXISTING TYPE C CURBS ONLY UPON APPROVAL BY CITY ENGINEER.
8. STANDARD CURB AND GUTTER MUST BE ROTATED ON SHED ROADS TO DRAIN TOWARD STREET.

1. A CONCRETE MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER PRIOR TO SCHEDULING POUR. CONCRETE TO HAVE 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI.
2. STRUCTURES SHALL CONFORM TO OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, CURRENT EDITION, SECTION 00759.
3. STANDARD SIDEWALK CROSS SLOPE SHALL BE 1.5% TOWARD THE STREET. WHEN THE ADJACENT PROPERTY IS LOWER THAN THE TOP OF THE CURB, SIDEWALK MAY SLOPE AWAY FROM STREET.
4. DRAIN BLOCKOUTS IN THE CURB SHALL BE EXTENDED TO THE BACK OF THE SIDEWALK WITH A 3" DIA. PLASTIC PIPE AT A 1.5% SLOPE. A CONTRACTION JOINT SHALL BE PLACED OVER THE PIPE. NO COUPLINGS UNDER SIDEWALK. OUTFALL AT FACE OF CURB EQUALS GUTTER PAN ELEVATION.
5. SEE STANDARD WHEELCHAIR/BICYCLE RAMP DETAILS ODOT STANDARD DRAWING RD744, CITY STANDARD DRAWING T-212.
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7. WRAP PROTRUDING STRUCTURES IN SIDEWALKS WITH PREFORMED EXPANSION JOINT FILLER.
8. REINFORCING STEEL SHALL BE 2 INCHES FROM THE BOTTOM OF THE CONCRETE SLAB.



12/13/2023



2/12/2024

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RECORD DRAWING




CITY OF NEWPORT
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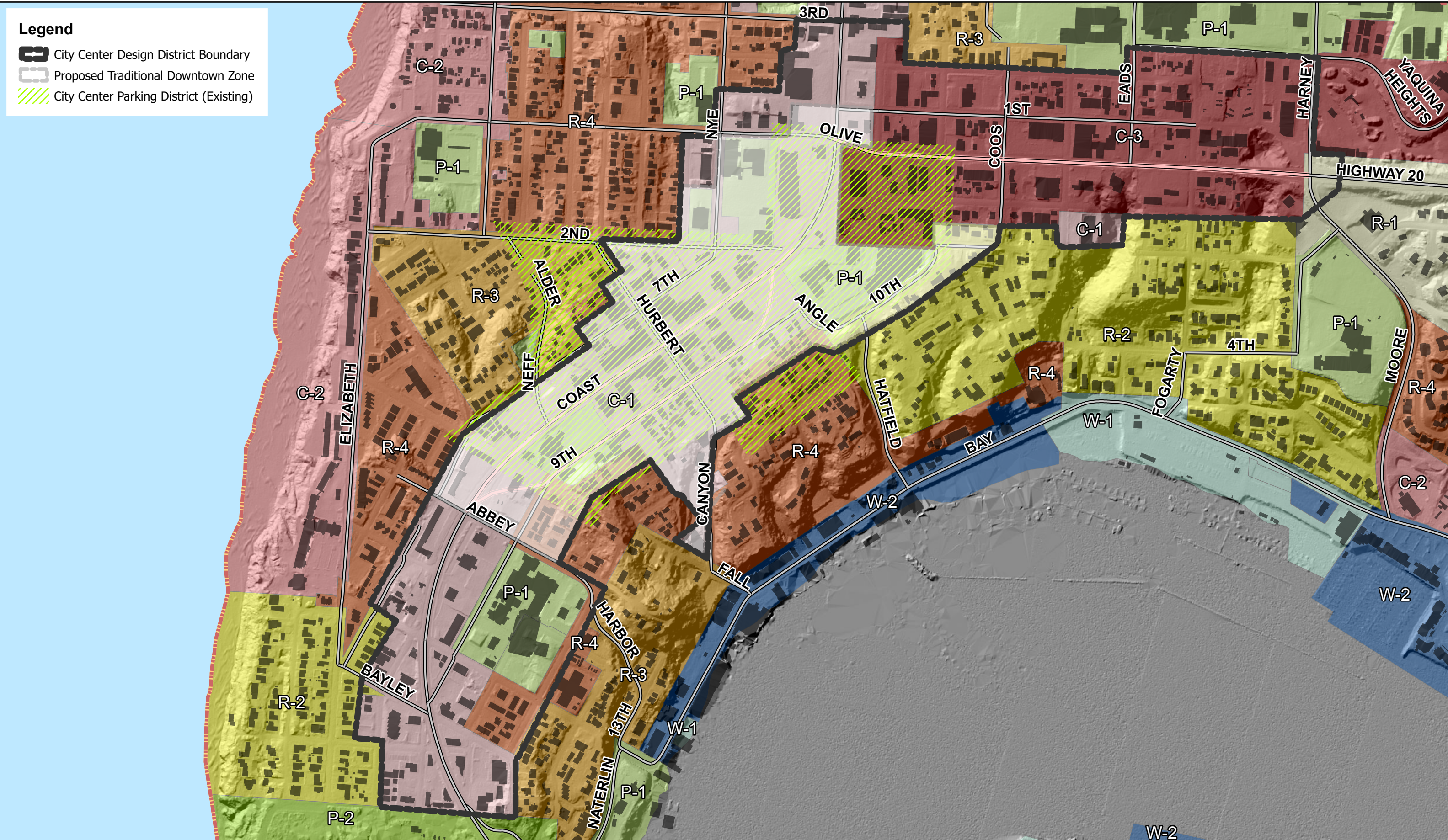
**BAYFRONT ABANDONED DRIVEWAY
APPROACH AND SIDEWALK IMPROVEMENTS**

106 AND 103 SW BAY BLVD

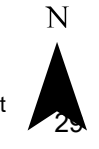
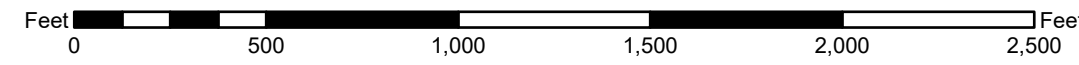
D-4

Legend

-  City Center Design District Boundary
-  Proposed Traditional Downtown Zone
-  City Center Parking District (Existing)



City Center Revitalization Plan Parking District Options



City Center Design Review District Form Based Code

Planning
Commission Work
Session

July 28



AGENDA

- Orientation to the packet
- What is and why use a Form Based approach?
- Overview of the Form Based Code
- Newport Municipal Code (NMC) Amendments
- Discussion questions

ORIENTATION TO THE PACKET

- Discussion questions – page 6 of your packet
- Form Based Approach (introduction) – page 11 of your packet
- Applicability – page 13
- Regulating Plan – page 15
- Public Realm Standards (Street Cross Sections) – page 21
- Development and Design Standards – page 29
- Amendments to the Newport Municipal Code – page 66

Discussion questions – page 6

This document, the City Center Design Review District Form Based Code, represents the Newport Municipal Code (NCM) amendments necessary to implement the City of Newport City Center Revitalization Plan, and fulfills the TGM grant-funded project tasks related to code amendments.

Discussion

This document will be presented to the Newport Planning Commission at their July 2025 work session. On this and the following pages are issues for work session discussion.

“Retail Ready” Frontage Standard

The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready Frontage Types is detailed on pages 40-43. Two alternative options for discussion are:

- » Only on side streets which have been identified as retail-focused streets– SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
- » On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

“Retail Ready” Frontage Standard. The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready” Frontage Types are detailed on pages 40-43. Two alternative options for discussion are:

1. Only on side streets which have been identified as retail-focused streets— SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
2. On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

Encroachments. The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

DISCUSSION QUESTIONS

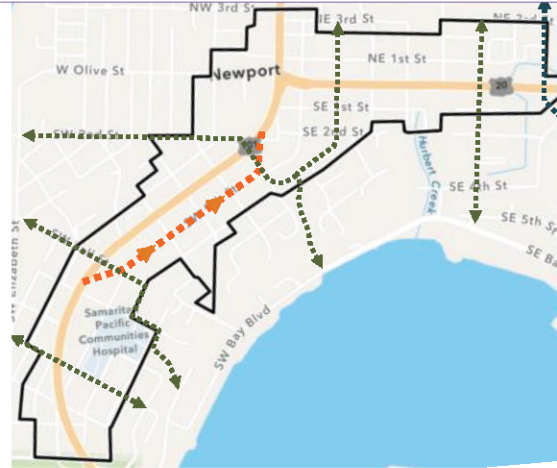
Architectural Features.

The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

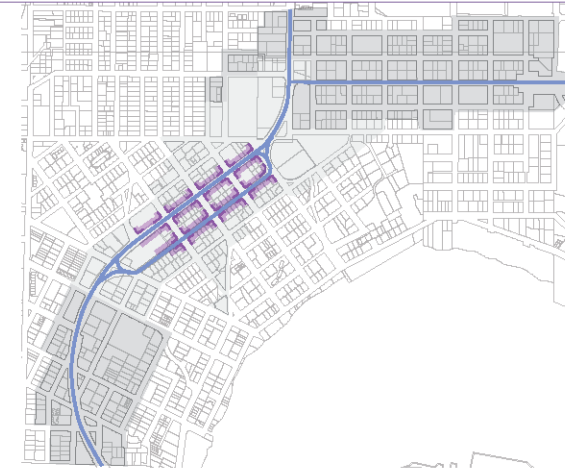
1. Keep the current generic listing (see “Detailed Design,” page 34).
2. Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.

Form Based Approach – page 11



City Center Design Review District (CCDRD)

The City Center Design Review District (CCDRD) is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.



CCDRD Form Based Code (FBC)

This document translates the vision created during the public involvement process into a series of land use, streetscape, and urban design strategies, using a form based approach.

Form based codes (FBCs) are development regulations used by local government agencies that emphasize the physical character of development and deemphasize the categorization of land uses and activities. They provide greater predictability about the look and feel of development and offer developers a clearer understanding of what the community seeks. In return, FBCs can make it easier for citizens to help create the physical development they want, which will more likely lead to their acceptance of development and street designs in their community.

The new 14.30 City Center Design Review District and amendments to the municipal code and Comprehensive Plan will:

FBC Approach

Promote a “form based” approach that emphasizes building shape, size, and relationship to streets and open spaces and de-emphasizes land use.

Use a “Regulating Plan” to translate the CCARP vision into a plan and map of the project area. The regulating plan will show where different development and design standards apply.

Utilize clear and objective dimensional standards. An example of a dimensional standard is “front yard setback – 20 feet.”

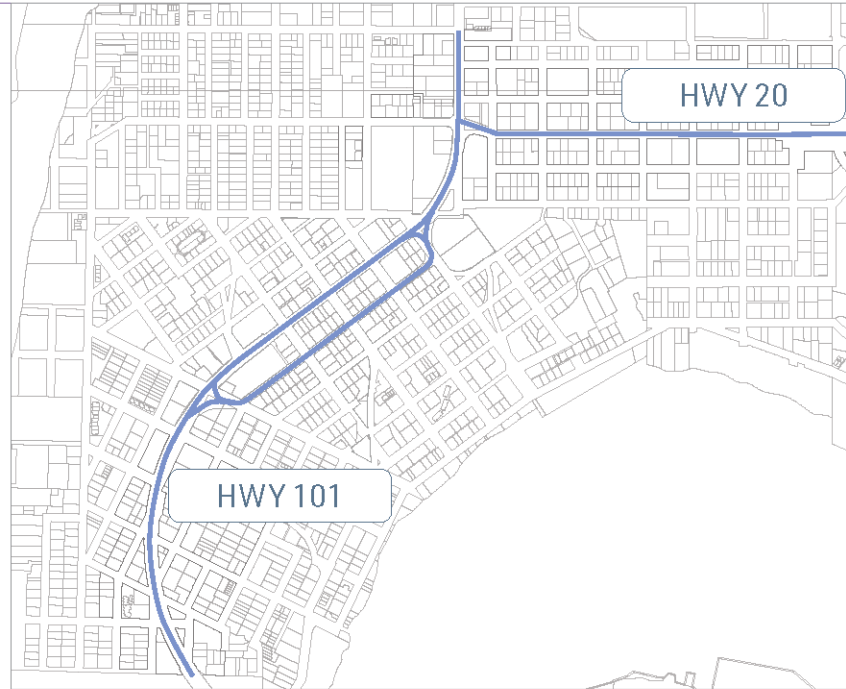
In general, provide as much information in tables as possible to aid clarity of the provisions and use graphics to communicate the intent of the development standards and provisions..

Display development and design standards in tables. The standards in the table will be keyed to the Regulating Plan, Frontage Types, or other diagrams.

Provide purpose or intent statements and visual examples to help explain the intended outcome.

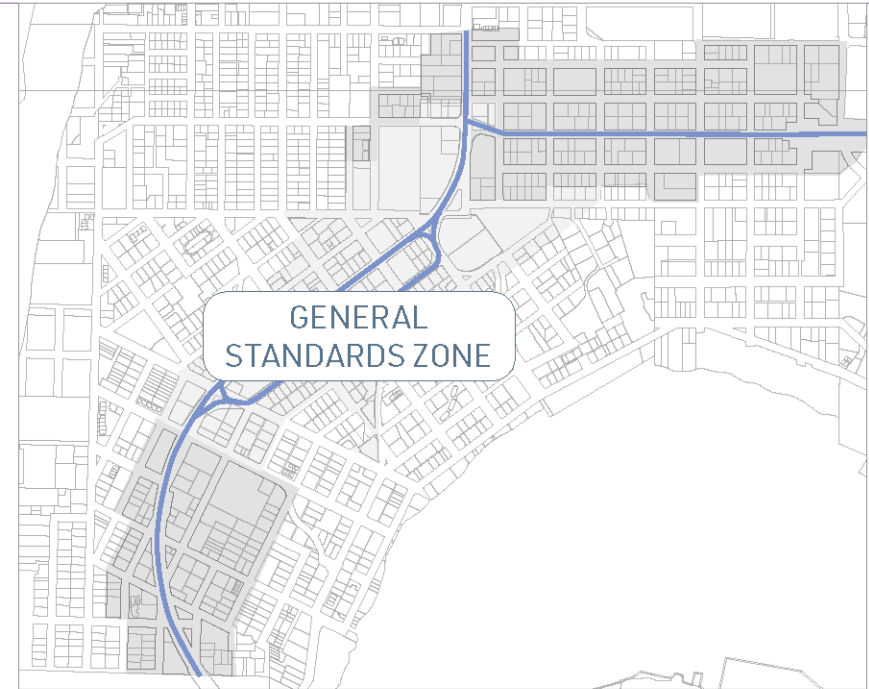
APPLICABILITY

Applicability – page 13



Public Realm Standards

Street Cross Sections (Section 04)

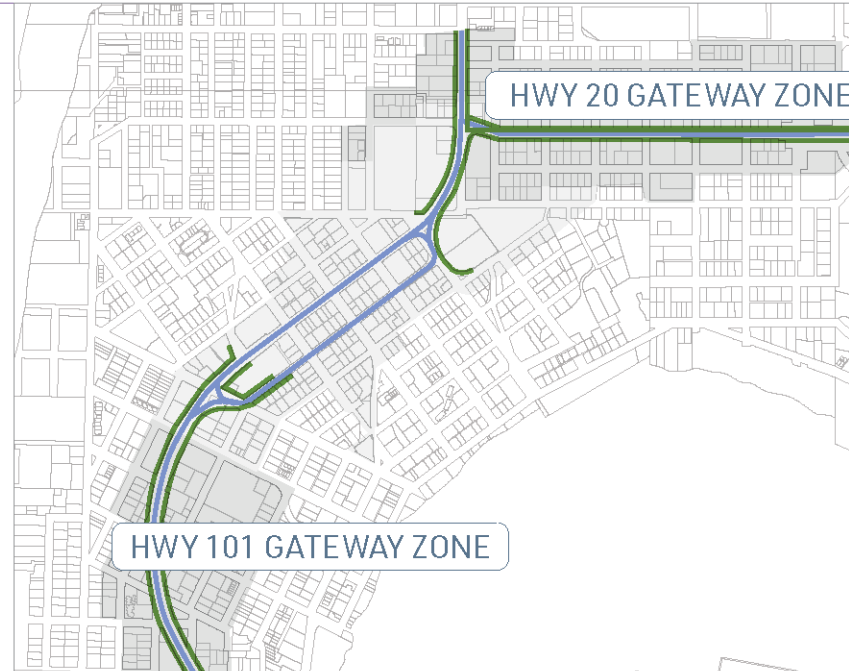


General Standards

Street Cross Sections (Section 04)

General Standards (Section 05)

APPLICABILITY

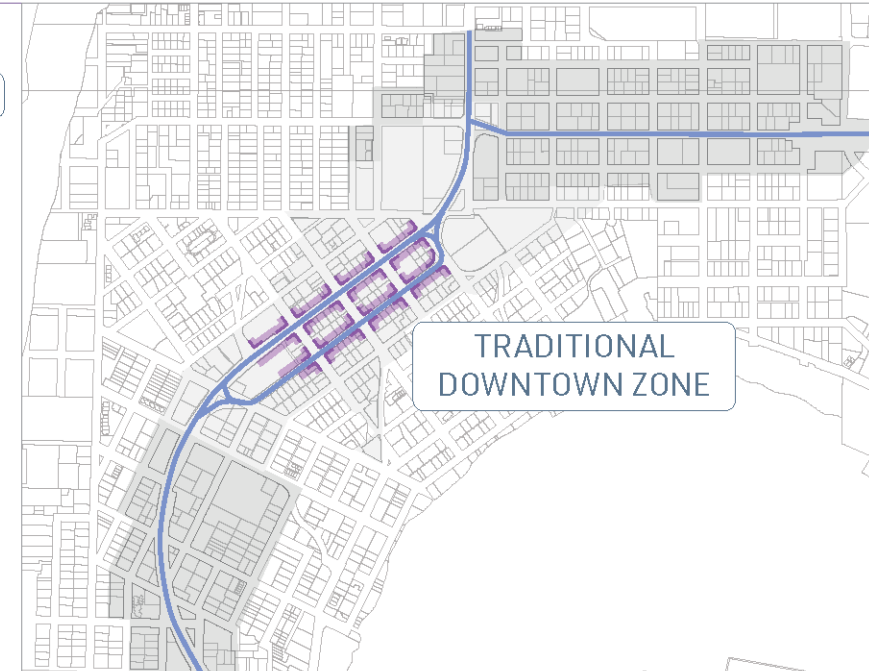


Gateway Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)



Traditional Downtown Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

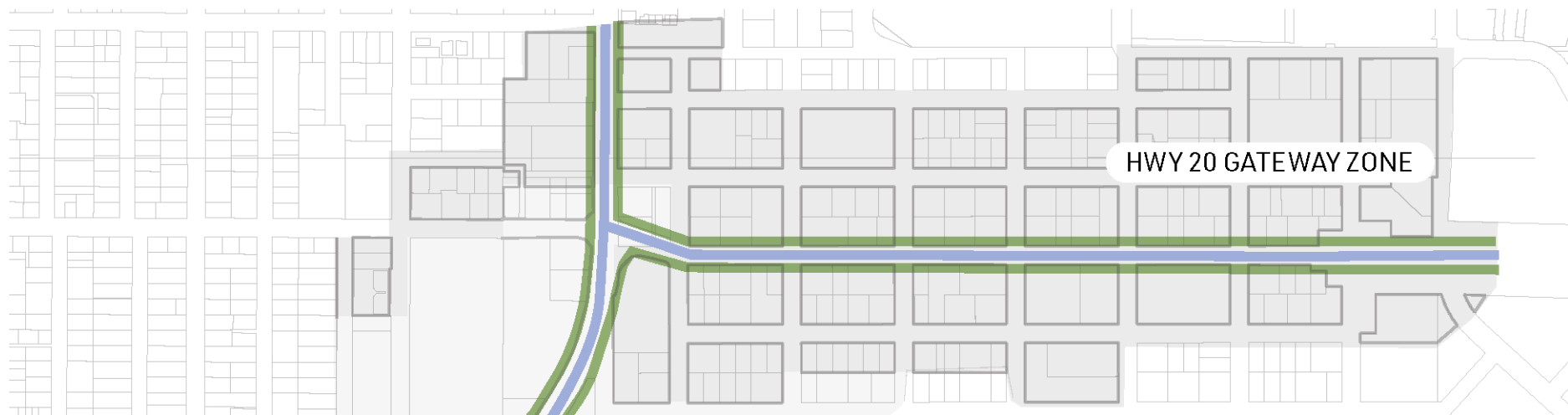
Gateway Frontage Types (Section 5)

Traditional Downtown Frontage Types (Section 5)

HWY 20 GATEWAY ZONE

Regulating Plan

– page 15




LEGEND

 URBAN FRONTAGE REQUIRED

 RETAIL-READY FRONTAGE REQUIREMENT

 LANDSCAPE FRONTAGE REQUIRED

 GENERAL STANDARDS APPLY

 PRIMARY STREET

HWY 101 GATEWAY ZONE



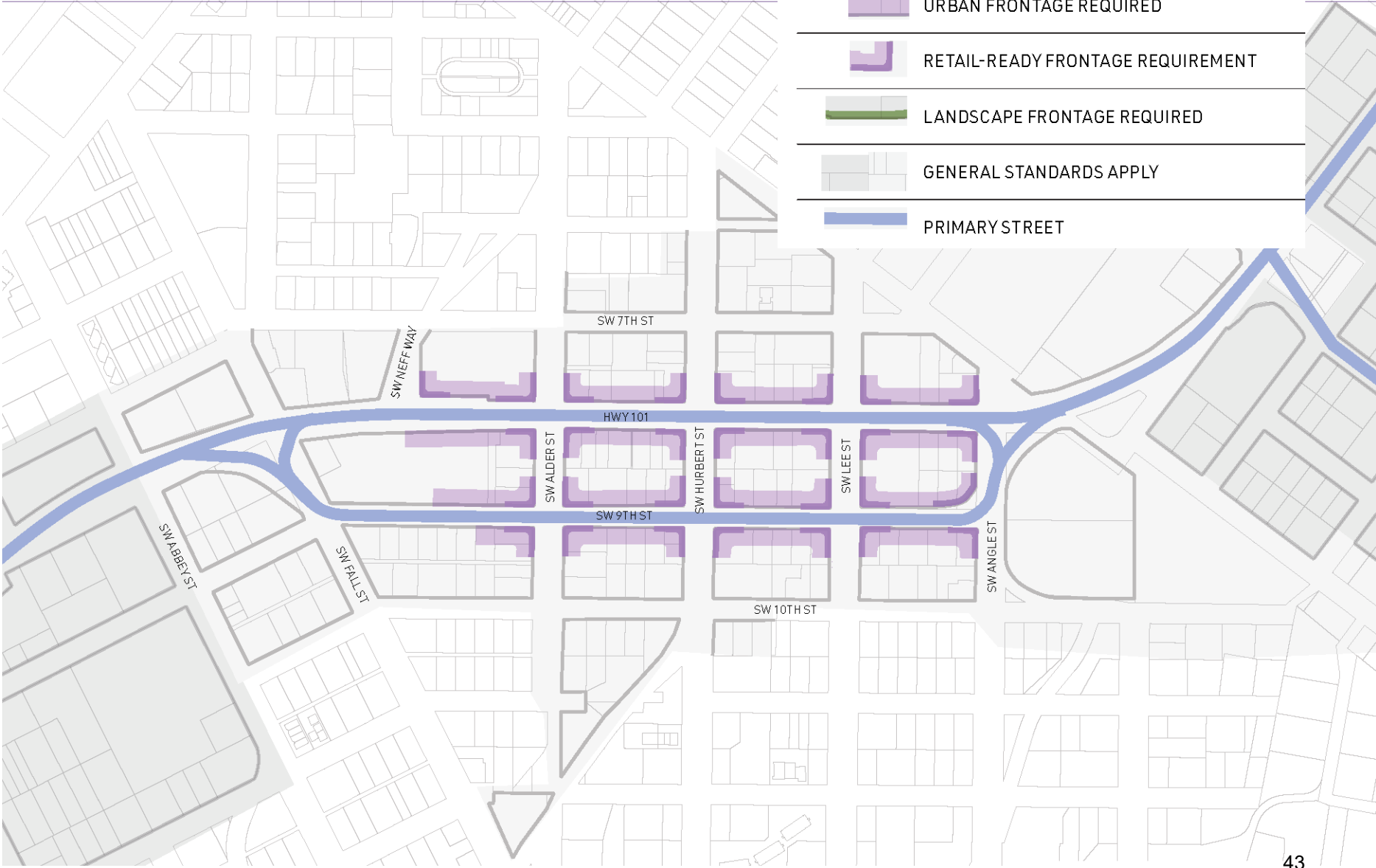
LEGEND

	URBAN FRONTAGE REQUIRED
	RETAIL-READY FRONTAGE REQUIREMENT
	LANDSCAPE FRONTAGE REQUIRED
	GENERAL STANDARDS APPLY
	PRIMARY STREET

TRADITIONAL DOWNTOWN ZONE

LEGEND

- URBAN FRONTAGE REQUIRED
- RETAIL-READY FRONTAGE REQUIREMENT
- LANDSCAPE FRONTAGE REQUIRED
- GENERAL STANDARDS APPLY
- PRIMARY STREET



APPLICABLE ODOT URBAN BLUEPRINT STANDARDS

Public Realm Street Standards (Street Cross Sections) – page 22

	CCARP CORE - TRADITIONAL DOWNTOWN		CCARP GATEWAY - URBAN MIX	
PUBLIC REALM STANDARDS	HWY 101	SW 9TH ST	HWY 20 / SW OLIVE ST	
Pedestrian realm (feet)	14 or greater	12 or greater	14 or greater (ideal)	12 or greater
Frontage / Pedestrian / Buffer / Curb-gutter (feet)	<u>2 / 8 / 5 / .5</u>	0 / 8 / 4 / 0	0 / <u>8</u> / 4 / 0	0 / 8 / 4
Transition realm (feet)	16 (one side) / 8	8 (one side only)	0	none
Bike / Buffer / Parking (feet)	<u>5 / 3 / 8 / 0</u>	5 / 3 / 0	0 / 0 / 0	
Travelway realm (feet)	22	22	36	36
Travel lane / Left turn lane and/or Raised Median (feet)	<u>11 / 11</u>	<u>11 / 11</u>	12 / 12 / 12	12 / 12 / 12
Totals (feet)	74	54	60	60

Key | [Critical dimensions](#)

STREET CROSS SECTIONS

Hwy 101 – Southbound couplet

Traditional Downtown Main Street

Supports residential and retail uses at the ground level.

Aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Shopfront (left) and Urban Frontage – Residential Porch-Stoop-Terrace, (right), see page 40.

SW 9th St. – Northbound couplet

Traditional Downtown Secondary Street

Generally supports residential and some retail uses at the ground level.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



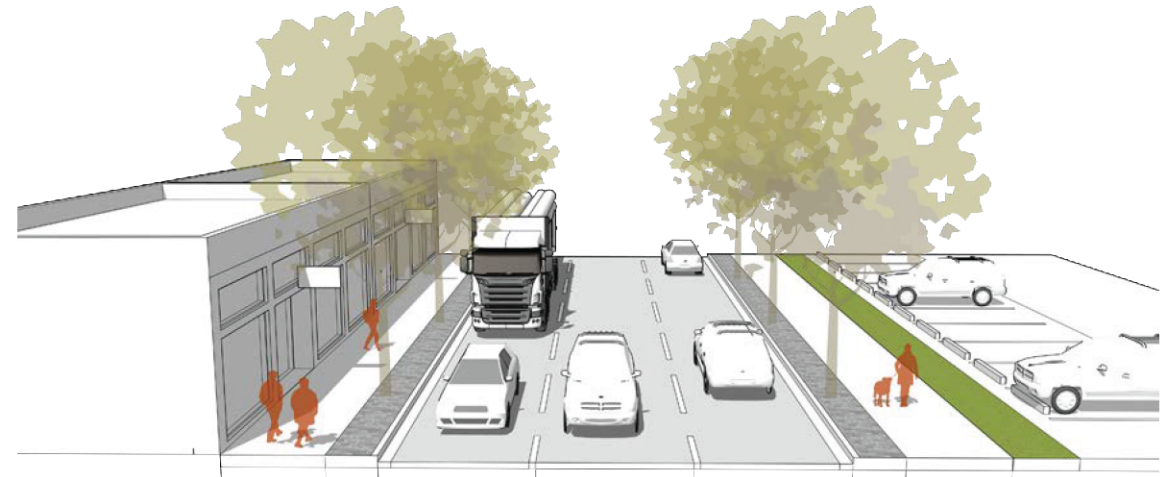
STREET CROSS SECTIONS

Hwy 20 – Olive St.

Improved pedestrian realm supports residential and some retail uses at the ground level.

Street environment and function will be improved with the consolidation of driveways on Olive Street; business access will be relocated to a single, central location on each block face or lot, and on side streets.

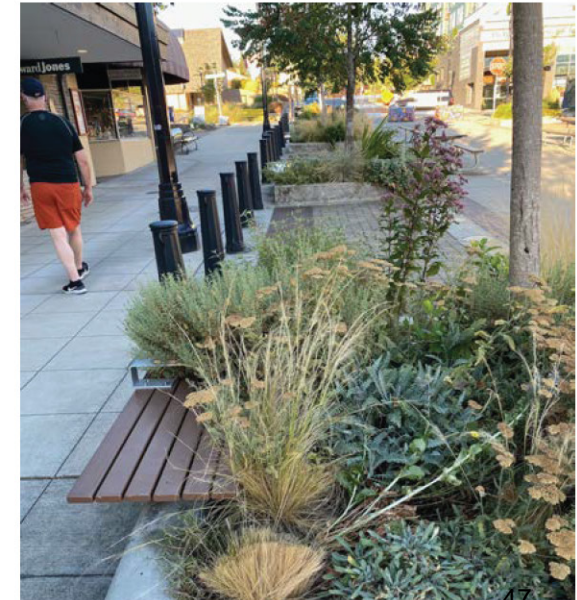
Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



STREET CROSS SECTIONS

SW Hurbert St.

Parklet Street



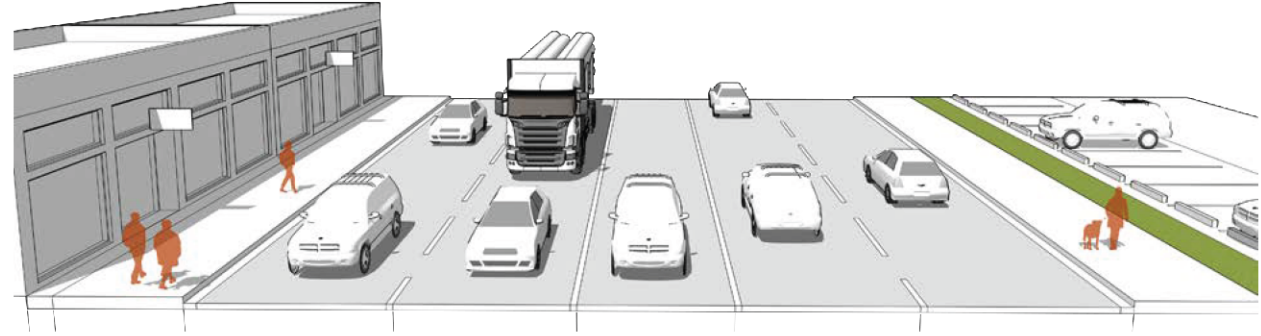


STREET CROSS SECTIONS

Hwy 101 –South of the couplet

Does not align with ODOT Blueprint for Urban Design Urban Mix recommendations.

Unimproved pedestrian realm and lack of on-street parking does not support retail, however, public realm and development requirements (e.g., FBC frontage types, General Standards) represent significant enhancements to Hwy 101 S.



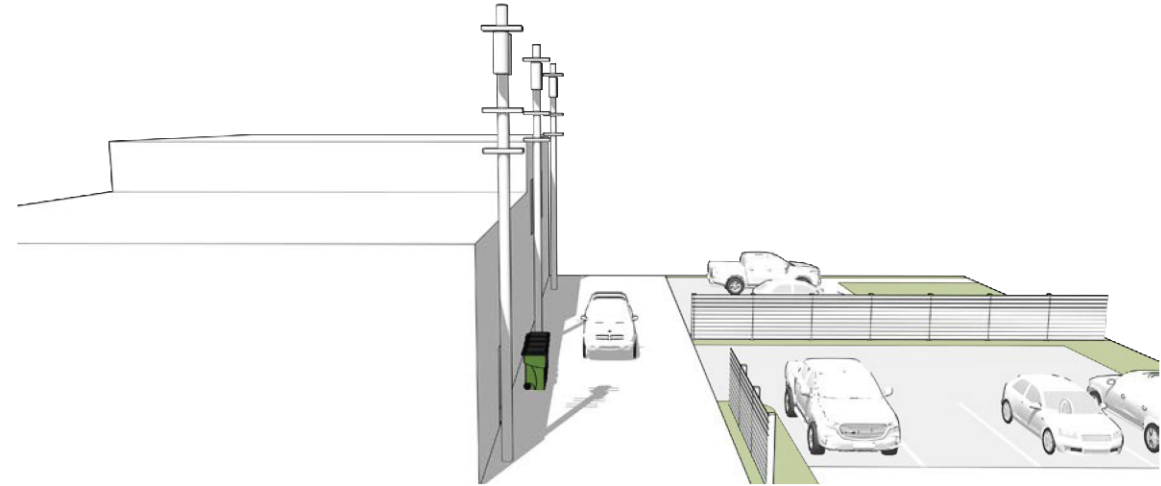
Cross Section showing existing conditions within existing right of way with Landscape Frontage Type applied (right) and General Design Standards applied (left).

Cross section enhancements and FBC frontage zone requirements represent opportunities for Hwy 101 Gateway Zone and hospital-related redevelopment enhancements.

STREET CROSS SECTIONS

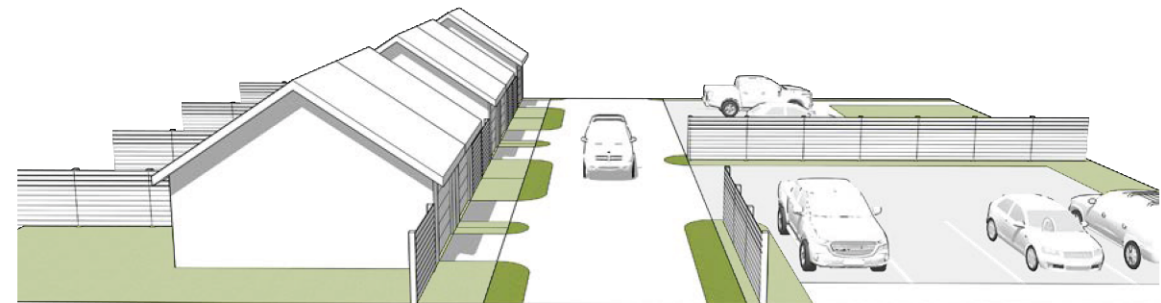
Commercial Alley

Private or public



Residential Alley

Private or public



Development and Design Standards – page 30

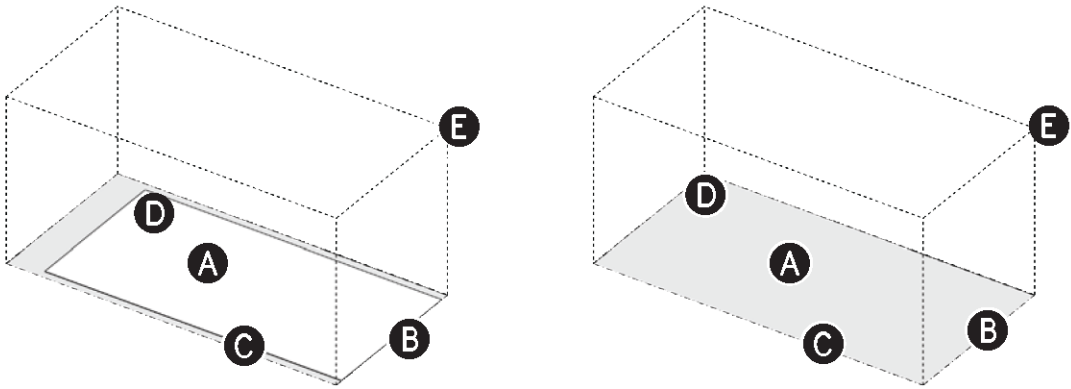
A suite of clear and objective standards:

- Development standards – lot size, width, setbacks, lot coverage, building height, and maximum density (land area required per dwelling unit);
- Clear and objective design standards for street-facing building façades or façades facing other public spaces (e.g., cottage cluster common courtyard), and
- Frontage Types - extra design standards for the first twenty feet of buildings within the Traditional Downtown (as defined by the Regulating Plan); includes options and choices for compliance.

SUMMARY OF BASE ZONE DEVELOPMENT STANDARDS

The base zone development standards will apply to new development along with the new form based standards.

Building envelope standards are summarized in the table.



14.13.010 DENSITY LIMITATIONS	C-1 AND C-3	P-1
A Minimum lot size (square feet)	5,000	5,000
B Minimum lot width (feet)	25	25
C Setbacks – Front and 2nd Front / Side / Rear (feet)	0 / 0 / 0	0 / 0 / 0
D Maximum lot coverage (%)	85-90	100
E Maximum building height (feet)	50	50
Density – Land Area Required per Unit (sq. feet)*	1,250 *	1,250 *

Roof slopes shall be between 5:12 and 12:12 except for shed roof additions or shed dormers may be 3:12.

14.30.030 – CCDRD General Standards

A. Purpose

The design standards for the City Center Design Review District require a minimum level of design on every building. These standards are intended to promote district character, attention to detail, human-scale and pedestrian-oriented design, while affording flexibility to use a variety of architectural styles.

City Center development is intended to implement the CCARP community vision and must address the following design objectives:

- Articulation – All street-facing buildings must incorporate design elements that break up façades into smaller planes.
- Eyes on the street – A certain percentage of the area of each street-facing façade must be windows.
- Main entrance – On street-facing façades, at least 1 main entrance must meet standards for location, orientation, and visibility.
- Detailed design – All street-facing buildings must include several features selected from a menu.

B. Applicability

Design Standard	Where it applies
Articulation – 14.30.030(BC)(1)	Applicable to dwellings facing the street when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.
Windows – 14.30(C)(2)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.
Main entrance – 14.30(C)(3)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.
Detailed design – 14.30(C)(4)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.

Design Standard	Where it applies
Transitional space – 14.30(C)(5)	Applicable to ground floor dwellings with access from the street or shared open space (e.g. a common courtyard), and access entry door is: (a) Within 10 feet of the street-facing property line, or (b) Within the front yard setback, or (c) Within 10 feet of a shared open space common tract or easement.
Pedestrian circulation – 14.30(C)(6)	Applicable to the entire site, and Applicable only for new buildings.
Off-street parking – 14.30(C)(7)	Applicable to clustered parking where parking spaces are 5 or more.

The design standards in this subsection apply to the types of development listed below when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.

1. New dwellings.
2. Expansions of structures in that add area to any street-facing façade. The design standards for such expansions are applicable as follows:
 - a. Expansions that add 75 square feet or less of street-facing façade area are exempt from all design standards.
 - b. Expansions that add more than 75 square feet and less than 200 square feet of street-facing façade area

FRONTAGE TYPES

BUILD-TO LINE / FRONTAGE LINE

FRONT,

EXAMPLES

SHOPFRONT



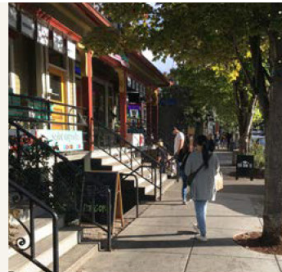
Shopfront frontage is characterized by a façade that is aligned close to the frontage line with the building entrance at sidewalk grade.

FORECOURT



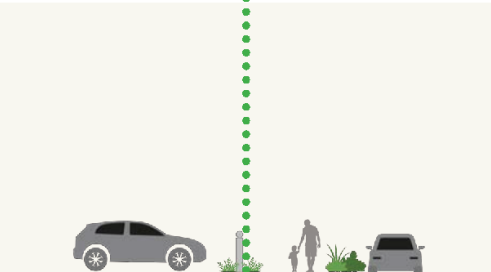
Forecourt frontage is characterized by portions of the façade close to the frontage line alternated by portions of the façade which are set back.

PORCH-STOOP-TERRACE



Porch-stoop-terrace frontage is characterized by a façade that is aligned close to the frontage line with the first story set back from the frontage line with a porch, a stoop, or a terrace.

LANDSCAPE FRONTAGE 1, 2, AND 3



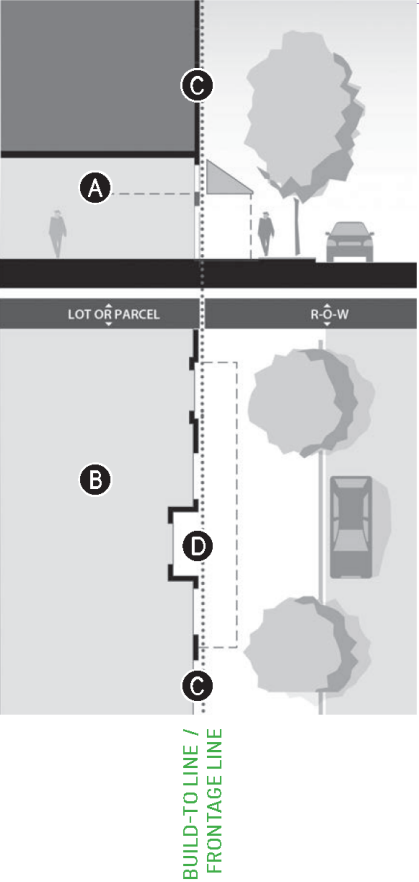
Landscape frontage 2 is characterized by a low wall, fence, and/or vertical landscaping such as a hedge that is aligned with the frontage line.

URBAN Frontages

Urban Frontage – Shopfront

FINAL DRAFT

FRONTAGE TYPES



URBAN FRONTAGE – SHOPFRONT

The Shopfront Frontage is characterized by a façade that is built up to the Build To Line or Frontage Line which is typically the front lot line.

The building entrance shall be at sidewalk grade. The Shopfront Frontage shall have substantial glazing on the ground floor. Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

- | | | |
|---|---|----|
| A | “Retail ready” minimum ground floor height (feet) | 18 |
| B | “Retail ready” minimum ground floor depth (feet) | 20 |
| C | Building Setback from Build-to Line (feet) | 0 |

Retail ready ground floor shall be constructed to meet commercial building standards.

- | | |
|---|--|
| D | Building entries shall provide a canopy or awning and/or be recessed behind the front building façade. |
|---|--|

Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.



Retail-ready shopfronts



Bay window and awning encroachments into the Right of Way

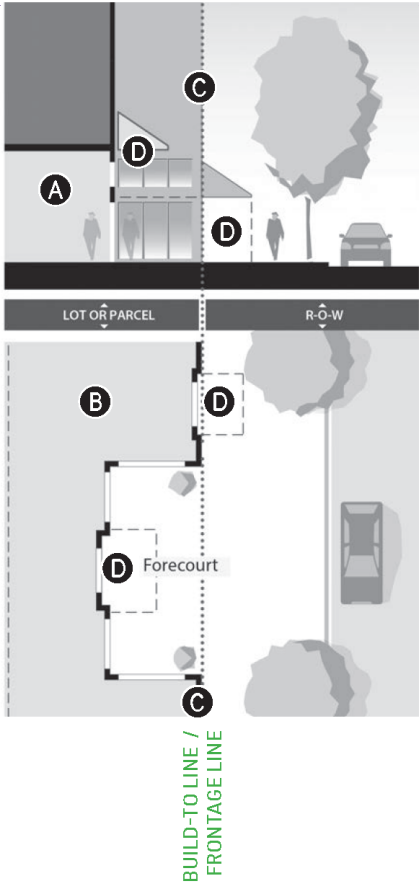


Recessed entry

Urban Frontage – Forecourt

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FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

- A** “Retail ready” minimum ground floor height (feet) 18
- B** “Retail ready” minimum ground floor depth (feet) 20
- C** Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

- C** Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.
 - Vertical separation – Distance from ground: Minimum 18 inches; maximum 3 feet;
 - Horizontal separation – Distance from Build To Line: Minimum 3 feet; maximum 15 feet
- D** Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.



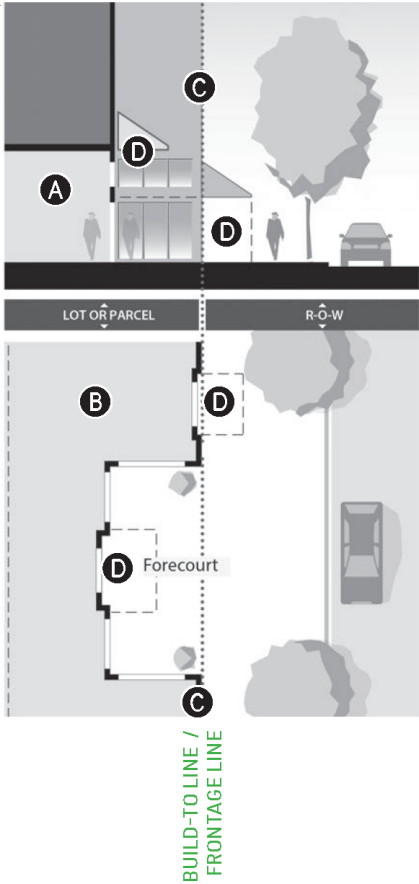
Forecourts shared by multiple retail establishments and restaurants



Urban Frontage – Forecourt

FINAL DRAFT

FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

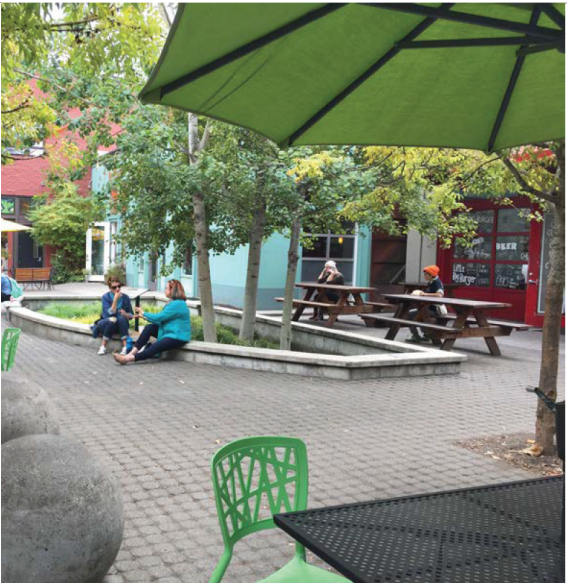
Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

- A** “Retail ready” minimum ground floor height (feet) 18
- B** “Retail ready” minimum ground floor depth (feet) 20
- C** Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

- C** Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.
 - Vertical separation – Distance from ground: Minimum 18 inches; maximum 3 feet;
 - Horizontal separation – Distance from Build To Line: Minimum 3 feet; maximum 15 feet
- D** Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

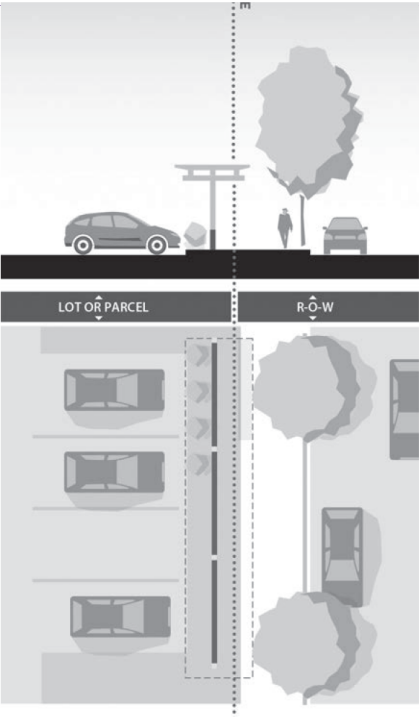


Forecourts shared by multiple retail establishments and restaurants



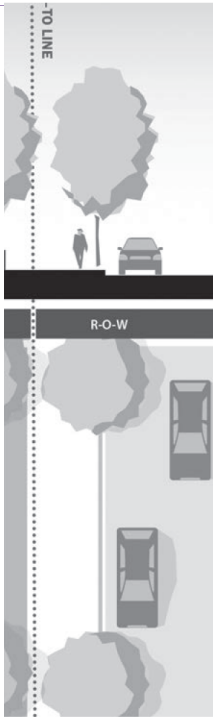
Landscape Frontages

FRONTAGE TYPES



LANDSCAPE FRONTAGE 1 - LOW WALL AND TRELLIS

A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet	5
B Height of Structure or Planting, feet	The underside of the Trellis portion of a Low Wall and Trellis shall be a minimum of 8 feet above grade and a maximum of 14 feet above grade. The Low Wall portion of a Low Wall and Trellis shall be a minimum of 1.5 feet and a maximum of 3 feet and have a minimum depth of 1.5 feet.
C Materials	The Trellis shall be heavy timber or steel (or a similar material) and shall consist of open structure with no decking or awning material. The Trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center. The Low Wall shall be wood, masonry, and/or concrete.
D Openings	Openings in the Low Wall and Trellis are allowed for pedestrian pathways, sidewalks, plazas, and driveways.
E Surface Parking Setback	Surface Parking shall be set back a minimum of 3 feet from the Low Wall and Trellis.
F Ground Cover and Planting	The area between the Build-to Line and the Trellis shall be hardscaped with either masonry pavers or stamped concrete. The setback between the Low Wall and surface parking shall be planted with low shrubs, groundcover, and climbing plants.

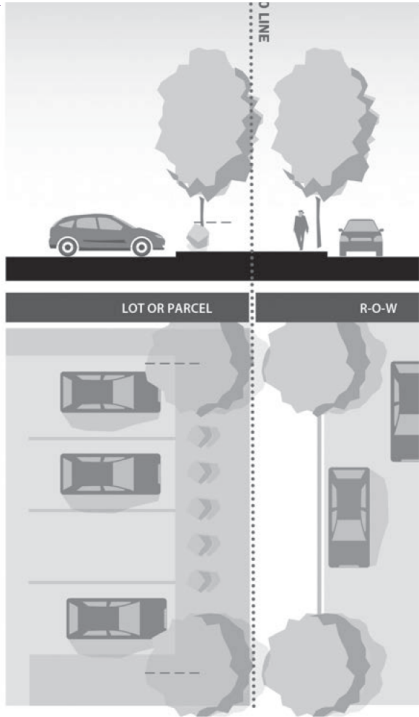


LANDSCAPE FRONTAGE 2 - URBAN FENCE OR WALL

A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet	5
B Height of Structure or Planting, feet	The fence or the wall shall be at least 2 feet high and no more than 3 feet high.
C Materials	Walls shall be wood masonry, and/or concrete; fences shall be made of wrought iron, steel, or a similar material (but not chain-link) and must be dark in color. Fences may be no more than 50% sight obscuring.
D Openings	Openings in the Urban Fence or Wall are allowed for pedestrian pathways, sidewalks, plazas, and driveways.
E Surface Parking Setback	The surface parking area shall be set back, at a minimum, an additional 5 feet to provide room for required landscaping and stormwater infiltration and/or retention.
F Ground Cover and Planting	In addition to the required fence or wall, trees and shrubs shall be provided. One large tree is required every 30 linear feet minimum. The shrubs shall be at least as high as the wall or fence, and shall be no more than 6 feet high.

Landscape Frontages

FRONTAGE TYPES



LANDSCAPED FRONTAGE 3 - LOW HEDGE

A Vertical Landscaping or Structure Setback from Build-to Line, minimum feet	5
B Height of Structure or Planting, feet	The shrubs shall be a minimum of 3 feet high. If a low wall is provided in place of shrubs it shall be a minimum of 3 feet high.
C Materials	The surface parking area shall be screened with a continuous row of hedges or shrubs immediately adjacent to the parking area, except where there is a driveway. Shrubs must be mostly opaque year round. A low wall may be substituted for the shrubs but the trees and groundcover plants are still required.
D Openings	Openings in the Landscape Frontage are allowed for pedestrian pathways, sidewalks, plazas, and driveways.
E Surface Parking Setback	10 feet minimum
F Ground Cover and Planting	In addition to the required shrubs, one large tree is required every 30 linear feet. The shrubs/hedge shall be interrupted with a gap of up to 2 feet wide in order to accommodate trees.

ES



DRAFT

**Newport City Center Design Review District – Amendments to the
Newport Municipal Code**

Contents

- Summary of Amendments Table
- Track change amendments

Summary of Amendments

Title XIV – Zoning (Chapters 14.03 through 14.30)

Chapter	Subsection	Description of Amendments or Discussion
14.03.050 – Residential Uses		Footnote to land use table listing for R-2, to permit Cottage Clusters, Multifamily uses, and Rooming and Boarding Houses in the CCDRD.
14.03.070 – Commercial and Industrial Uses		Footnotes to the land use table listings for C-1 and C-3: <ul style="list-style-type: none">· To permit residential uses in the CCDRD on C-1- and C-3-zoned land, and· To permit residential uses on the ground floor of buildings in the CCDRD.
14.03.100 – Public Uses		Footnote to land use table listing for P-1, to permit residential uses in the CCDRD on P-1-zoned land.

Chapter	Subsection	Description of Amendments or Discussion
14.06.060 – Recreational Vehicle Parks		Added language to make limitations on Recreational Vehicle Parks the same for CCDRD as they currently are for Historic Nye Beach Design Review District.
14.09.050 – Mobile Food Units		Added language to make limitations on Mobile Food Units the same for City Center parking district as they currently are for Historic Nye Beach parking district: only permitted if it is a Mobile Food Unit Pod.
14.13.010 – Density Limitation	NMC 14.13.020 - Table “A”	<p>Table “A” manages the lot size, density, form, shape, and size of buildings by land use district. Exceptions for special areas such as Nye Beach Design Review District, are referenced in the footnotes.</p> <p>Similar to the way that special zoning and design standards that apply to Nye Beach Design Review District is established in footnote #4, a new footnote is added to reference the special zoning standards that apply to the City Center Design Review District (CCDRD):</p> <p><i>Special Zoning Standards apply to C-1, C-3, P-1, R-2, and R-4 zoned property within the City Center Design Review District as outlined in NMC 14.30.200.</i></p> <p>Where the standards in NMC 14.30.200 differ from those in Table A, the standards in 14.30.200 will govern development form in the CCDRD.</p>

List of amendments:

- 14.03.050 – Residential Uses. Footnote to land use table listing for R-2, to permit Cottage Clusters, Multifamily uses, and Rooming and Boarding Houses in the CCDRD.
- 14.03.070 – Commercial and Industrial Uses. Footnotes to the land use table listings for C-1 and C-3, to permit residential uses in the CCDRD on C-1- and C-3-zoned land, and to permit residential uses on the ground floor of buildings in the CCDRD.
- 14.03.100 – Public Uses. Footnote to land use table listing for P-1, to permit residential uses in the CCDRD on P-1-zoned land.
- 14.15.020 – Residential Uses in Nonresidential Zoning Districts. Multiple text changes to ensure that residential uses are permitted outright use, subject to the CCDRD FBC provisions.
- 14.30.020 –(B) – Design Review Districts: Overlay Zones Established. New text establishing the CCDRD and describing the boundaries.
- 14.30.030 – Adoption of Design Review: Guidelines and Standards. References the “City Center Design Review District Form Based Code ” and states that the Regulating Plan, Public Realm Standards, and Development and Design Standards, contained therein shall be the standards applicable to the City Center Design Review District.

List of amendments, continued:

- 14.30.060 – Approval Authority. Adds the City Center Design Review Form Based Code to those documents subject to .060, which establishes that the approval or denial of a Design Review application is a ministerial action performed concurrent with review of a building permit by the Community Development.
- 14.30.100 – Special Zoning Standards in Design Review Districts. Adds the CCDRD to areas subject to special zoning standards and references the CCDRD Form Based Code. Prohibits drive through windows in the area defined as Traditional Downtown Zone within the CCDRD FBC.

DISCUSSION QUESTIONS

“Retail Ready” Frontage Standard. The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready” Frontage Types are detailed on pages 40-43. Two alternative options for discussion are:

1. Only on side streets which have been identified as retail-focused streets— SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
2. On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION QUESTIONS

Encroachments. The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

DISCUSSION QUESTIONS

Architectural Features.

The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

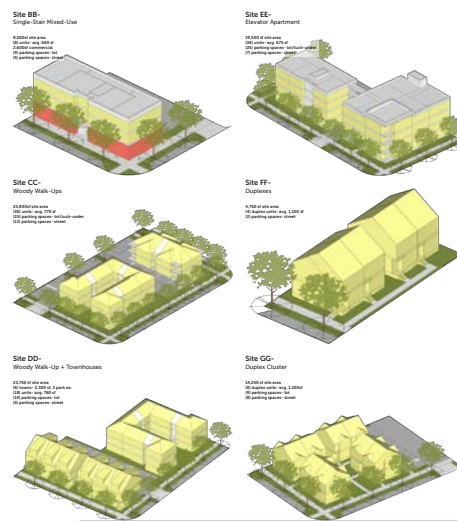
1. Keep the current generic listing (see “Detailed Design,” page 34).
2. Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.



Thank you

City Center Design Review District Form Based Code

THE NEWPORT CITY CENTER REVITALIZATION PLAN PROJECT
FINAL DRAFT, JUNE 2025



bruary 2025



u r b s w o r k s

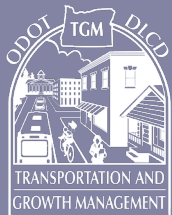
Newport City Center Revitalization Plan Project

City Center Design Review District Form
Based Code

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04	01 INTRODUCTION
06	02 FORM BASED APPROACH
10	03 REGULATING PLAN
16	04 PUBLIC REALM
24	05 DEVELOPMENT AND DESIGN STANDARDS
38	06 REFERENCES

APPENDIX A | NEWPORT MUNICIPAL CODE (NMC) AMENDMENTS



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The contents of this document do not necessarily reflect views or policies of the State of Oregon.

David Evans and Associates Inc.

ECOnorthwest

This document, the City Center Design Review District Form Based Code, represents the Newport Municipal Code (NCM) amendments necessary to implement the City of Newport City Center Revitalization Plan, and fulfills the TGM grant-funded project tasks related to code amendments.

Discussion

This document will be presented to the Newport Planning Commission at their July 2025 work session. On this and the following pages are issues for work session discussion.

“Retail Ready” Frontage Standard

The Regulating Plan (Section 3) currently shows “Retail-Ready” Frontage Type required on Hwy 101 and SW 9th, at all corners within the Traditional Downtown Zone. The “retail-ready Frontage Types is detailed on pages 40-43. Two alternative options for discussion are:

- » Only on side streets which have been identified as retail-focused streets— SW Hurbert, SW Alder, and SW Lee Streets—for the full length of the side street and wrapping the corners (e.g., 25 linear feet from each corner, or to the nearest property line, whichever is less).
- » On Hwy 101 only, (not on SW 9th) where 101 intersects with SW Hurbert, SW Alder, and SW Lee Streets and wrapping the corners (e.g., 25 linear feet from each corner or to nearest property line, whichever is less).

DISCUSSION, CONTINUED

Discretionary process and review standards

» *What is the discretionary process and review standards for deviating from the clear and objective, fixed, form-based dimensional standards in the form based code?*

The CCDRD fosters housing as an important aspect of the policy changes and implementation of the CCARP Revitalization Plan. Amendments to the base zone land use listings mean that housing is allowed (but not required) on any lot within in the CCDRD.

The CCDRD complies with the state's middle housing requirements and HB 2001 and associated rules. The rules do not allow housing to be subject to discretionary review, and any standards that a city applies to housing beyond those in the Middle Housing ORS or Model Code must be applied to all housing types.

Given that housing is not subject to discretionary approval, the only development in the CCDRD that could be subject to discretionary review would be non-residential projects.

To comply with the middle housing rules, the CCDRD General Standards and Frontage Types (Section 5) allow a high degree of flexibility and choices through menus or options. No specific architectural style is promoted, and flexibility to use a variety of architectural styles is explicitly stated in the general standards.

This means that, while there are clear and objective, fixed, form-based dimensional standards, there is a high degree of flexibility for non-residential projects as well as housing development.

If an alternative, discretionary approval process for non-residential projects is desired, an option can be provided.

Options are:

- » Develop an alternative, discretionary approval process for non-residential projects similar to that taken in Nye Beach design review district, i.e., using design guidelines, intent statements, and example approaches.
- » Do not provide an alternative process for non-residential projects. Such projects would follow the same clear and objective review process and be subject to the same process as housing (ministerial action performed concurrent with review of a building permit by the Community Development).

DISCUSSION, CONTINUED

Encroachments

The CCDRD proposed provisions say: “Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.” See Urban Shopfront Frontage Type, page 40, for an example.

Encroachments of the kind mentioned are desirable for urban vitality and interest, especially in the Traditional Downtown Zone. The city permits encroachments on a very limited basis.

Discuss how the city allows encroachments and how the intent for architectural features (such as bay windows), and weather protection (awnings, marquees), can be allowed within the CCDRD.

Architectural Features

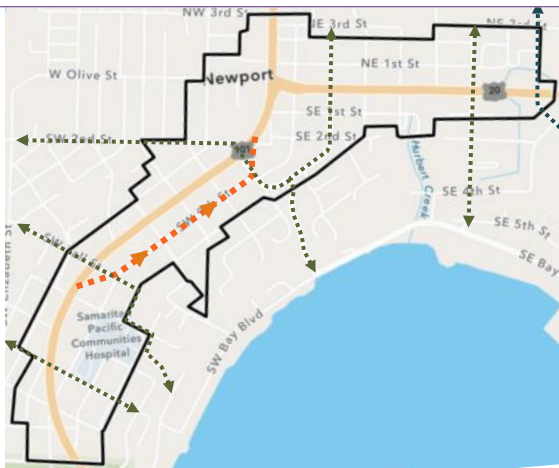
The CCDRD General Design Standards contain menus for architectural features but they are generic (see “Detailed Design,” page 34, for an example). The “Newport Design Review Glossary and Illustrations,” used for Historic Nye Beach Design Review District contains a more comprehensive listing along with illustrations. Besides being more comprehensive, these have the benefit of being in place for some time and are familiar to the community. While they are historic features many of them are timeless architectural features that can be rendered in a variety of architectural styles, from traditional to modern.

Options for discussion:

- » Keep the current generic listing (see “Detailed Design,” page 34).
- » Replicate or adapt certain features from the Newport Design Review Glossary and Illustrations, such as balcony, bay window, and/or turret. Combine these with the current generic listing.

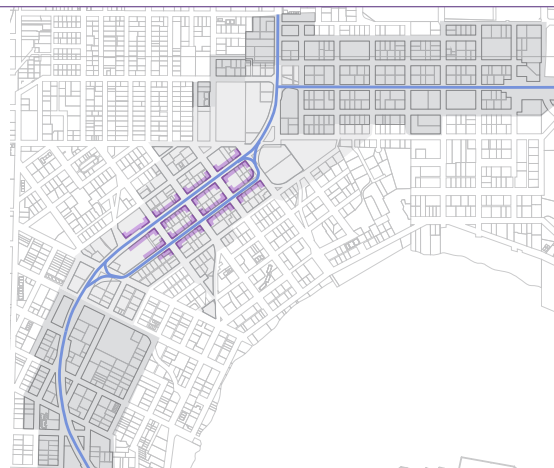
INTRODUCTION





City Center Design Review District (CCDRD)

The City Center Design Review District (CCDRD) is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.



CCDRD Form Based Code (FBC)

This document translates the vision created during the public involvement process into a series of land use, streetscape, and urban design strategies, using a form based approach.

Form based codes (FBCs) are development regulations used by local government agencies that emphasize the physical character of development and deemphasize the categorization of land uses and activities. They provide greater predictability about the look and feel of development and offer developers a clearer understanding of what the community seeks. In return, FBCs can make it easier for citizens to help create the physical development they want, which will more likely lead to their acceptance of development and street designs in their community.

FORM BASED APPROACH



The new 14.30 City Center Design Review District and amendments to the municipal code and Comprehensive Plan will:

FBC Approach

Promote a “form based” approach that emphasizes building shape, size, and relationship to streets and open spaces and de-emphasizes land use.

Use a “Regulating Plan” to translate the CCARP vision into a plan and map of the project area. The regulating plan will show where different development and design standards apply.

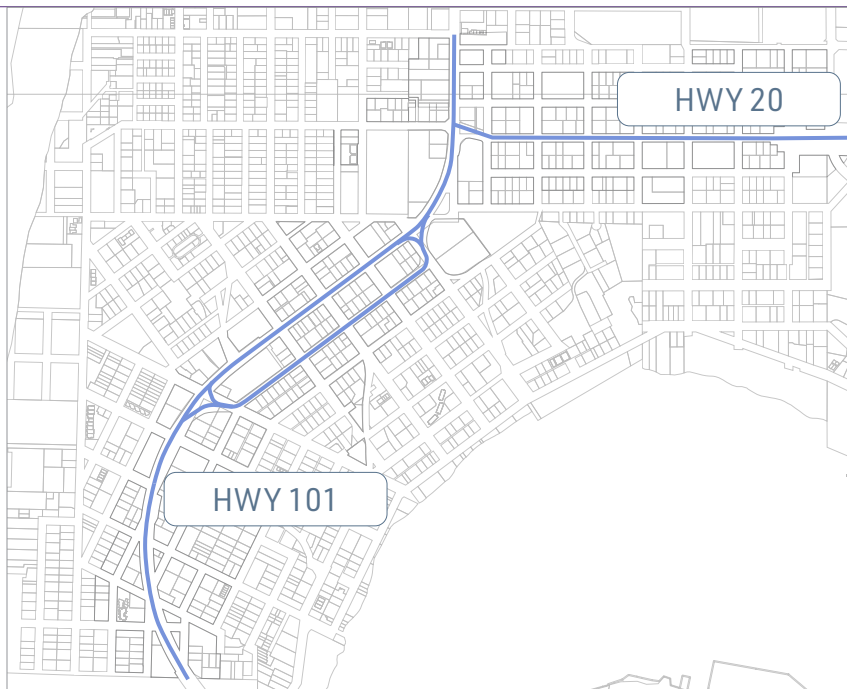
Utilize clear and objective dimensional standards. An example of a dimensional standard is “front yard setback – 20 feet.”

In general, provide as much information in tables as possible to aid clarity of the provisions and use graphics to communicate the intent of the development standards and provisions..

Display development and design standards in tables. The standards in the table will be keyed to the Regulating Plan, Frontage Types, or other diagrams.

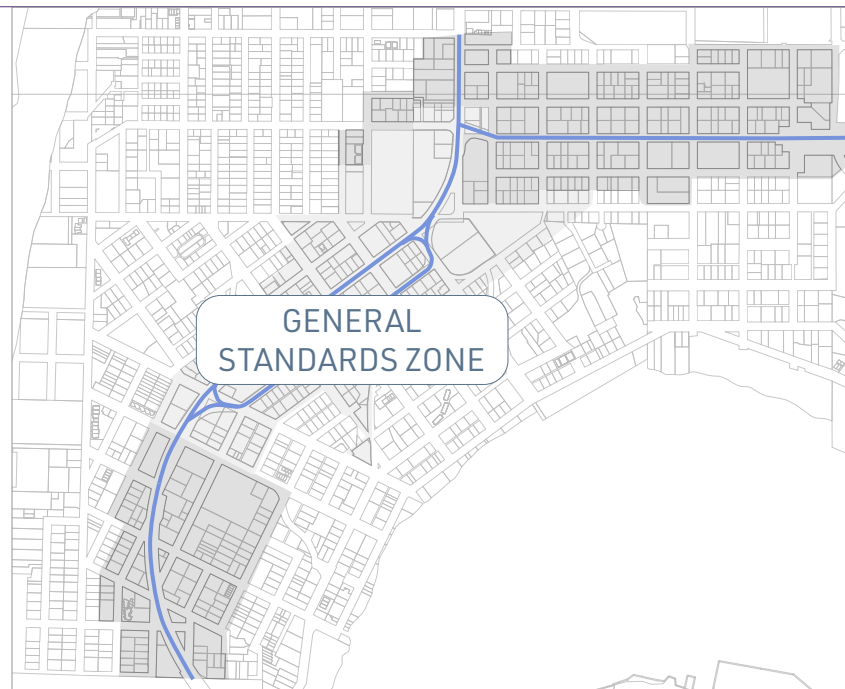
Provide purpose or intent statements and visual examples to help explain the intended outcome.

APPLICABILITY



Public Realm Standards

Street Cross Sections (Section 04)

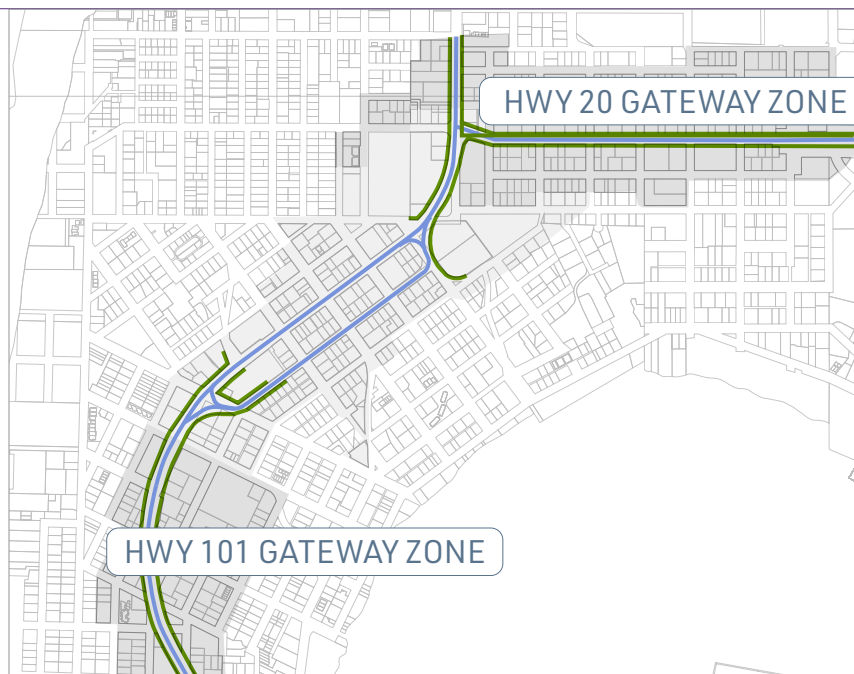


General Standards

Street Cross Sections (Section 04)

General Standards (Section 05)

APPLICABILITY

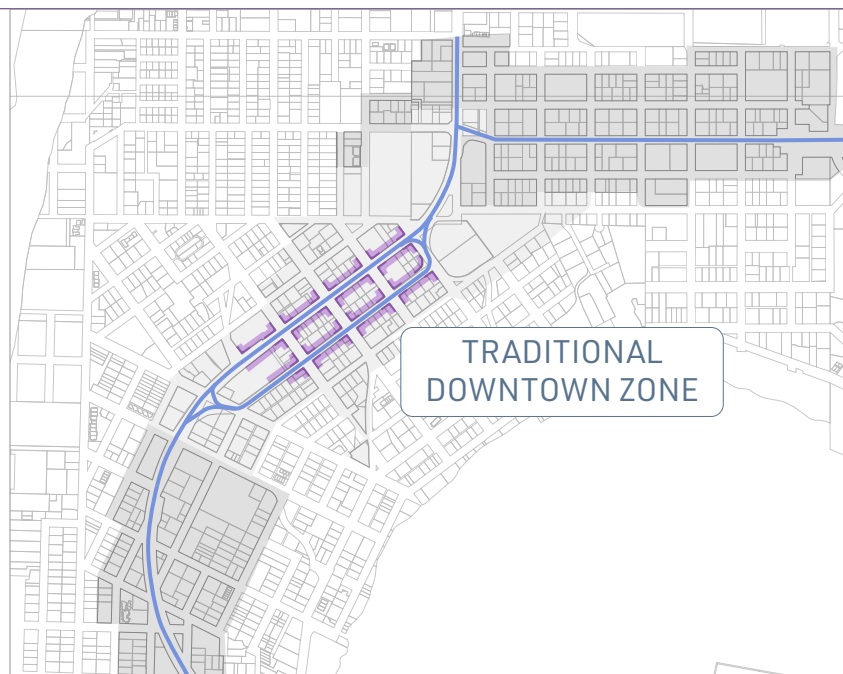


Gateway Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)



Traditional Downtown Frontage Types

Street Cross Sections (Section 04)

General Standards (Section 05)

Gateway Frontage Types (Section 5)

Traditional Downtown Frontage Types (Section 5)

REGULATING PLAN



Regulating Plan

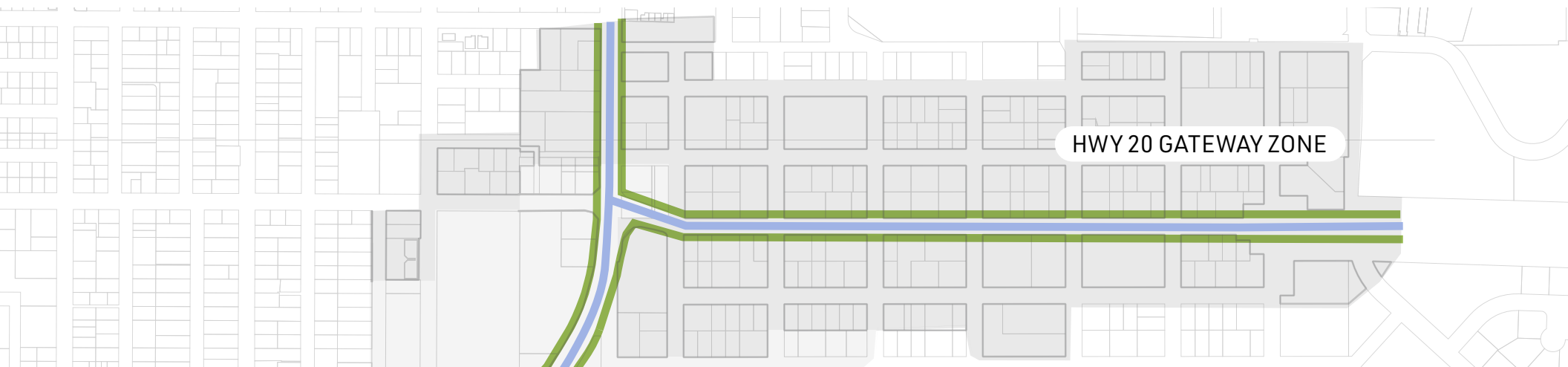
The purpose of the Regulating Plan is to translate the vision created during the public involvement process into an actionable plan and map. Each regulating plan component explains which areas are subject to specific regulations. The regulating plan is a critical tool to inform development of the Newport CCARP Design District and ensure the community vision is realized.

Regulating Plan Components

The regulating plan identifies mandatory components to be implemented through a combination of private development and public capital improvement projects. It provides flexibility for the Newport CCARP Design District to support a wide range of land uses and development types while ensuring strong connectivity, a form and character that aligns with the surrounding context, and effective placemaking and economic development to complement public investments on Hwy 101, SW 9th Street, and Hwy 20.

The Regulating Plan identifies requirements that are geographically located and apply regardless of the proposed development approach. This plan is supplemented by development standards that vary based on proposed development and land uses.

HWY 20 GATEWAY ZONE



LEGEND



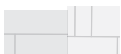
URBAN FRONTAGE REQUIRED



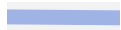
RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED



GENERAL STANDARDS APPLY



PRIMARY STREET

HWY 101 GATEWAY ZONE



LEGEND



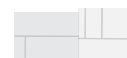
URBAN FRONTAGE REQUIRED



RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED

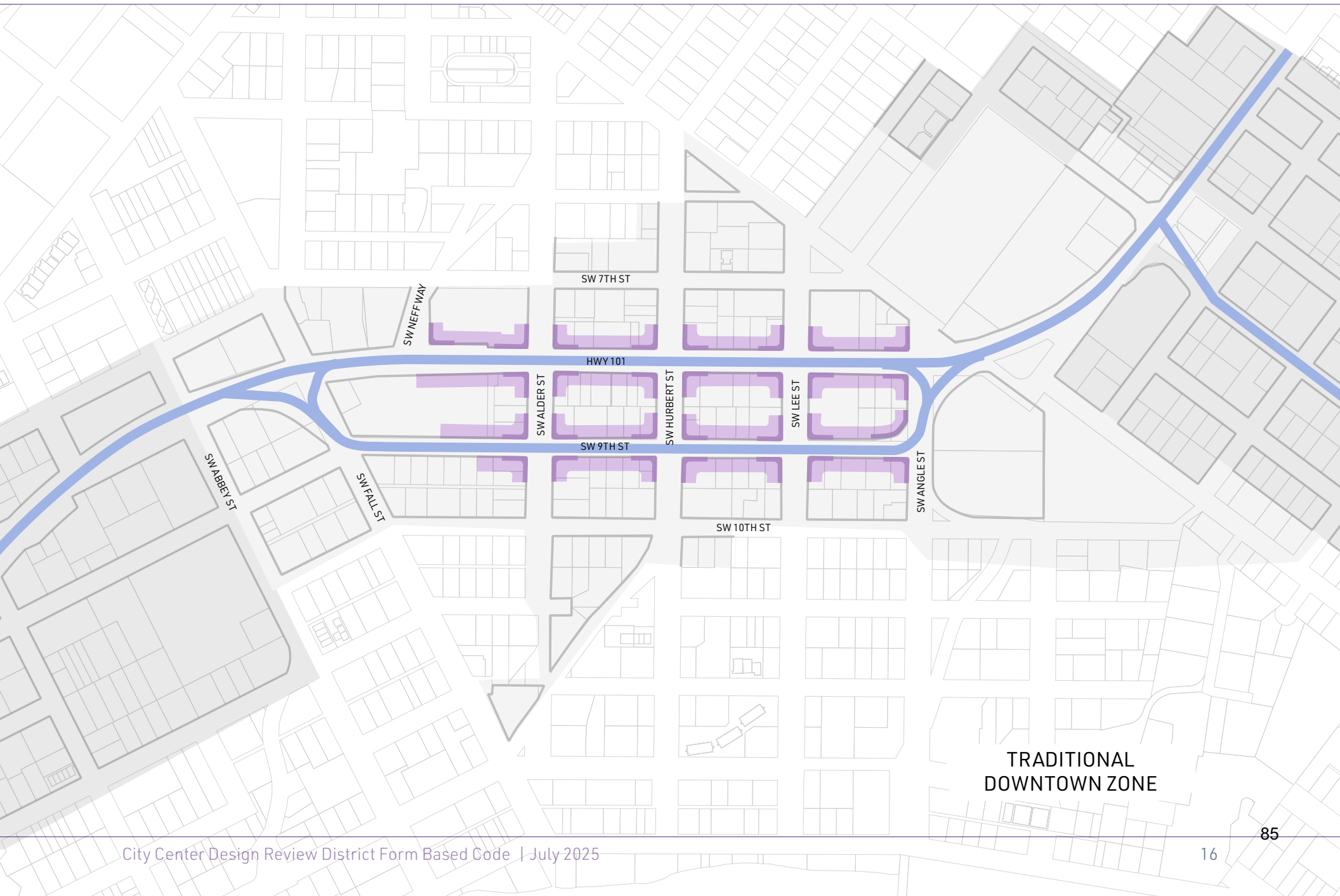


GENERAL STANDARDS APPLY



PRIMARY STREET

TRADITIONAL DOWNTOWN ZONE



TRADITIONAL DOWNTOWN ZONE

LEGEND



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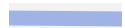
RETAIL-READY FRONTAGE REQUIREMENT



LANDSCAPE FRONTAGE REQUIRED



GENERAL STANDARDS APPLY



PRIMARY STREET



04

PUBLIC REALM

APPLICABLE ODOT URBAN BLUEPRINT STANDARDS

	CCARP CORE - TRADITIONAL DOWNTOWN		CCARP GATEWAY - URBAN MIX	
PUBLIC REALM STANDARDS	HWY 101	SW 9TH ST	HWY 20 / SW OLIVE ST	
Pedestrian realm (feet)	14 or greater	12 or greater	14 or greater (ideal)	12 or greater
Frontage / Pedestrian / Buffer / Curb-gutter (feet)	<u>2 / 8 / 5 / .5</u>	0 / 8 / 4 / 0	0 / <u>8 / 4</u> / 0	0 / 8 / 4
Transition realm (feet)	16 (one side) / 8	8 (one side only)	0	none
Bike / Buffer / Parking (feet)	<u>5 / 3 / 8</u> / 0	5 / 3 / 0	0 / 0 / 0	
Travelway realm (feet)	22	22	36	36
Travel lane / Left turn lane and/or Raised Median (feet)	<u>11 / 11</u>	<u>11 / 11</u>	12 / 12 / 12	12 / 12 / 12
Totals (feet)	74	54	60	60

Key | Critical dimensions

STREET CROSS SECTIONS

Hwy 101 – Southbound couplet

Traditional Downtown Main Street

Supports residential and retail uses at the ground level.

Aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Shopfront (left) and Urban Frontage – Residential Porch-Stoop-Terrace, (right), see page 40.

SW 9th St. – Northbound couplet

Traditional Downtown Secondary Street

Generally supports residential and some retail uses at the ground level.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



Cross Section illustrates Urban Frontage – Residential Porch-Stoop-Terrace, vertical separation standard (left); horizontal separation standard (right), see page 40.

STREET CROSS SECTIONS

Hwy 20 –Olive St.

Improved pedestrian realm supports residential and some retail uses at the ground level.

Street environment and function will be improved with the consolidation of driveways on Olive Street; business access will be relocated to a single, central location on each block face or lot, and on side streets.

Generally aligns with ODOT Blueprint for Urban Design Traditional Downtown / CBD Recommendations.



STREET CROSS SECTIONS

SW Hurbert St.

Parklet Street



STREET CROSS SECTIONS

SW Alder and SW Lee Streets

Festival Street or Plaza Street

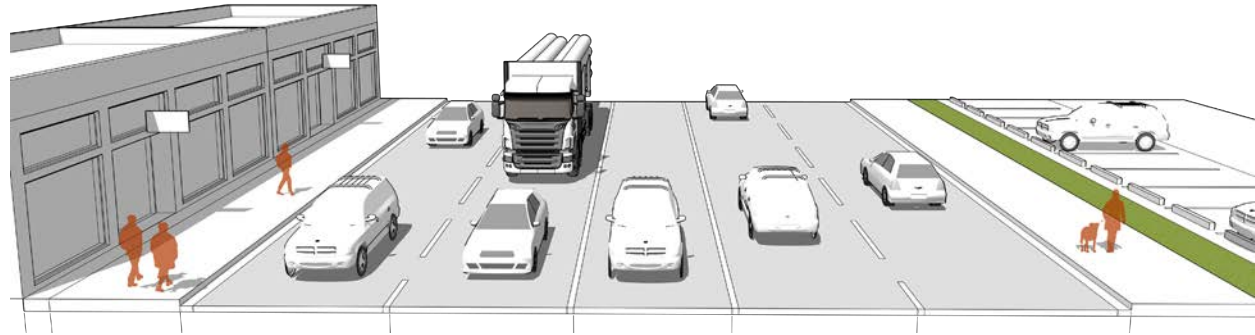


STREET CROSS SECTIONS

Hwy 101 –South of the couplet

Does not align with ODOT Blueprint for Urban Design Urban Mix recommendations.

Unimproved pedestrian realm and lack of on-street parking does not support retail, however, public realm and development requirements (e.g., FBC frontage types, General Standards) represent significant enhancements to Hwy 101 S.



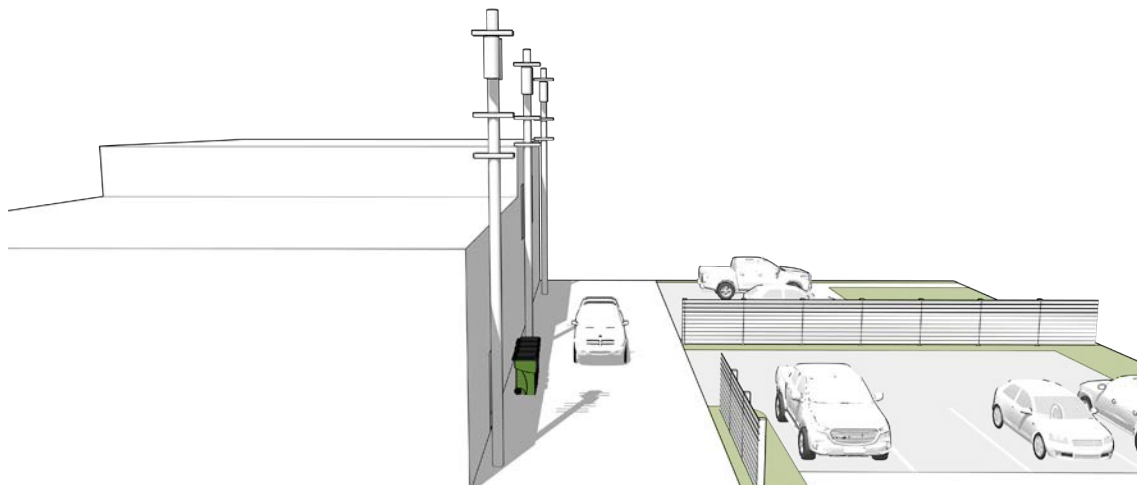
Cross Section showing existing conditions within existing right of way with Landscape Frontage Type applied (right) and General Design Standards applied (left).

Cross section enhancements and FBC frontage zone requirements represent opportunities for Hwy 101 Gateway Zone and hospital-related redevelopment enhancements.

STREET CROSS SECTIONS

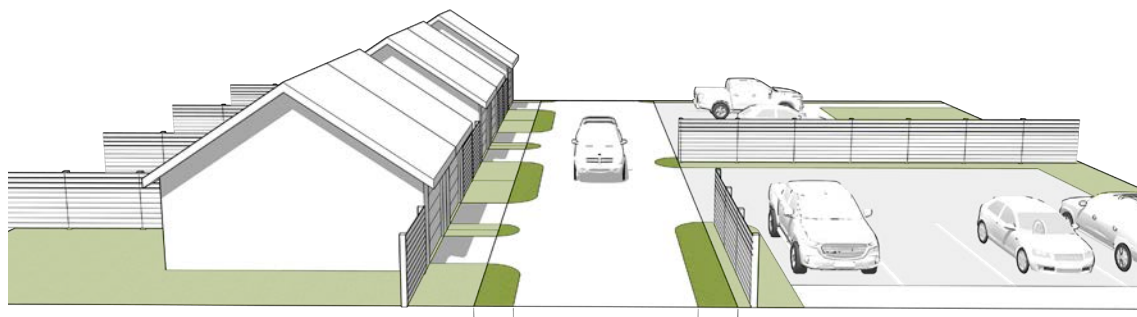
Commercial Alley

Private or public



Residential Alley

Private or public



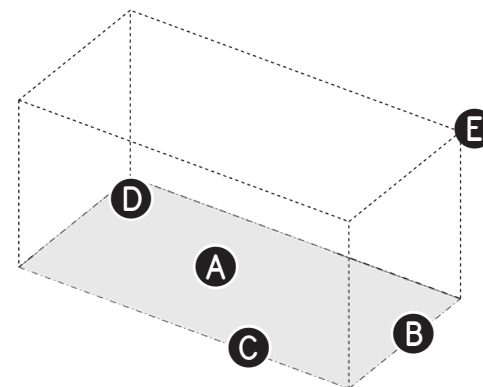
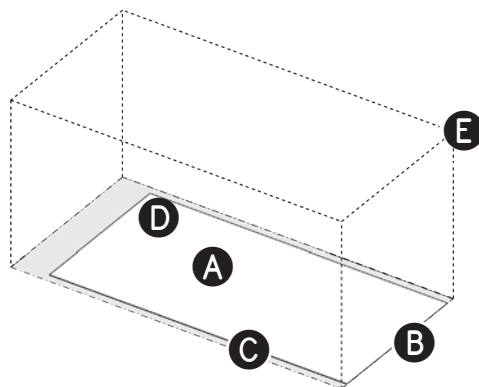
DEVELOPMENT AND DESIGN STANDARDS



SUMMARY OF BASE ZONE DEVELOPMENT STANDARDS

The base zone development standards will apply to new development along with the new form based standards.

Building envelope standards are summarized in the table.



14.13.010 DENSITY LIMITATIONS	C-1 AND C-3	P-1
A Minimum lot size (square feet)	5,000	5,000
B Minimum lot width (feet)	25	25
C Setbacks – Front and 2nd Front / Side / Rear (feet)	0 / 0 / 0	0 / 0 / 0
D Maximum lot coverage (%)	85-90	100
E Maximum building height (feet)	50	50
Density – Land Area Required per Unit (sq. feet)*	1,250 *	1,250 *

Roof slopes shall be between 5:12 and 12:12 except for shed roof additions or shed dormers may be 3:12.

GENERAL STANDARDS

14.30.030 – CCDRD General Standards

A. Purpose

The design standards for the City Center Design Review District require a minimum level of design on every building. These standards are intended to promote district character, attention to detail, human-scale and pedestrian-oriented design, while affording flexibility to use a variety of architectural styles.

City Center development is intended to implement the CCARP community vision and must address the following design objectives:

- Articulation – All street-facing buildings must incorporate design elements that break up façades into smaller planes.
- Eyes on the street – A certain percentage of the area of each street-facing façade must be windows.
- Main entrance – On street-facing façades, at least 1 main entrance must meet standards for location, orientation, and visibility.
- Detailed design – All street-facing buildings must include several features selected from a menu.

B. Applicability

Design Standard	Where it applies
Articulation – 14.30.030(BC)(1)	Applicable to dwellings facing the street when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.
Windows – 14.30(C)(2)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.
Main entrance – 14.30(C)(3)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.
Detailed design – 14.30(C)(4)	Applicable to dwellings facing the street, when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line, and Applicable to dwellings in a cluster or grouping, either facing a shared open space (e.g. a common courtyard) or a pedestrian path.

Design Standard	Where it applies
Transitional space – 14.30(C)(5)	Applicable to ground floor dwellings with access from the street or shared open space (e.g. a common courtyard), and access entry door is: (a) Within 10 feet of the street-facing property line, or (b) Within the front yard setback, or (c) Within 10 feet of a shared open space common tract or easement.
Pedestrian circulation – 14.30(C)(6)	Applicable to the entire site, and Applicable only for new buildings.
Off-street parking – 14.30(C)(7)	Applicable to clustered parking where parking spaces are 5 or more.

The design standards in this subsection apply to the types of development listed below when the closest wall of the street-facing façade is within 50 feet of a front or street side lot line.

1. New dwellings.
2. Expansions of structures in that add area to any street-facing façade. The design standards for such expansions are applicable as follows:
 - a. Expansions that add 75 square feet or less of street-facing façade area are exempt from all design standards.
 - b. Expansions that add more than 75 square feet and less than 200 square feet of street-facing façade area are subject to Subsection Section 14.30(B)(1), Eyes on the Street. The expanded façade area must meet

the standards of Section 14.30(B)(1), Eyes on the Street, without consideration of the original street-facing façade area.

- c. Expansions that add 200 square feet or more of street-facing façade area are subject to the following design standards:
 - 3. The entire street-facing façade shall comply with Section 14.30(B) P (2) Windows.
 - 4. Section 14.30(B)(3) Main Entrance is applicable if an expansion would create a new main entrance. No expansion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.
 - 5. Section 14.30(B)(1) Articulation is applicable for expansions that add 20 lineal feet or more to the length of the street-facing façade.
 - 6. Section 14.30(B)(4) Detailed Design is not applicable for expansions. However, no expansion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the Detailed Design standards.
 - 7. Multiple expansions are allowed within a 5-year period if the street-facing façade will comply with the design standards that would have been applicable if the expansions occurred at the same time.
 - 8. Remodels that convert an attached garage to a habitable residential space. When applicable, the design standards apply only to the street-facing façade of the garage being converted. The following design standards are applicable:
 - a. Section 14.30(B)(3) Main Entrance is applicable if the garage conversion would create a new main entrance. No conversion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.

- b. Section 14.30(B)4 Detailed Design is not applicable. However, no conversion shall bring the street-facing façade out of conformance, or further out of conformance if already nonconforming, with the design standard.

C. Standards

All buildings that meet the applicability provisions in Section 14.30(B) Applicability shall meet the following design standards. The graphics provided are intended to illustrate how development could comply with these standards and should not be interpreted as requiring a specific architectural style. An architectural feature may be used to comply with more than one standard.

1. Articulation. All buildings shall incorporate design elements that break up all street-facing façades into smaller planes as follows.
 - a. For buildings with 25 to 50 feet of street frontage, a minimum of 1 of the following elements shall be provided along the street-facing façades.
 - i. A porch at least 5 feet deep.
 - ii. A balcony that is at least 2 feet deep and is accessible from an interior room.
 - iii. A bay window that extends at least 2 feet wide.
 - iv. A section of the façade that is recessed by at least 2 feet deep and 6 feet long.
 - v. A gabled dormer.
 - b. For buildings with over 50 feet of street frontage, at least 1 element in Subsection 4.161(1) i.-v. above shall be provided for every 25 ft of street frontage.
 - c. Elements shall be distributed along the length of the façade so that there are no more than 25 feet between 2 elements.
 - d. For buildings with less than 25 feet of street frontage, the building articulation standard is not applicable.
2. Windows. At least 15% of the area of each street-facing façade must be windows.

- a. Windows used to meet this standard must be transparent and allow views from the building to the street. Glass blocks and privacy windows in bathrooms do not meet this standard.
 - b. Window area is considered the entire area within the outer window frame, including any interior window grid. Glazed portions of entrance doors count as window area.
 - c. Doors used to meet this standard must face the street or be at an angle of no greater than 45 degrees from the street.
 - d. Door area is considered the portion of the door that moves. Door frames do not count toward this standard.
3. Main entrance. At least 1 main entrance must meet both of the following standards.
- a. Be no further than 8 ft behind the longest street-facing wall of the building.
 - b. Face the street, be at an angle of up to 45 degrees from the street, or open onto a porch. If the entrance opens up onto a porch, the porch must meet all of these additional standards.
 - i. Be at least 25 sq ft in area with a minimum 4-ft depth.
 - ii. Have at least 1 porch entry facing the street.
 - iii. Have a roof that is no more than 12 ft above the floor of the porch.
 - iv. Have a roof that covers at least 30% of the porch area.
4. Detailed design. All buildings shall include at least 5 of the following features on any street-facing façade.
- a. Covered porch at least 5 ft deep, as measured horizontally from the face of the main building façade to the edge of the deck, and at least 5 ft wide.
 - b. Recessed entry area at least 2 ft deep, as measured horizontally from the face of the main building façade, and at least 5 ft wide.
 - c. Offset on the building face of at least 16 in from 1 exterior wall surface to the other.

- d. Dormer that is at least 4 ft wide and integrated into the roof form.
 - e. Roof eaves with a minimum projection of 12 in from the intersection of the roof and the exterior walls.
 - f. Roof line offsets of at least 2 ft from the top surface of 1 roof to the top surface of the other.
 - g. Tile or wood shingle roofs.
 - h. Horizontal lap siding between 3 to 7 in wide (the visible portion once installed). The siding material may be wood, fiber-cement, or vinyl.
 - i. Brick, cedar shingles, stucco, or other similar decorative materials covering at least 40% of the street-facing façade.
 - j. Gable roof, hip roof, or gambrel roof design.
 - k. Window trim around all windows at least 3 in wide and 5/8 in deep.
 - l. Window recesses, in all windows, of at least 3 in as measured horizontally from the face of the building façade.
 - m. Balcony that is at least 3 ft deep, 5 ft wide, and accessible from an interior room. For Townhouses this standard is 2 ft deep and 4 ft wide.
 - n. One roof pitch of at least 500 sq ft in area that is sloped to face the southern sky and has its eave line oriented within 30 degrees of the true north/south axis.
 - o. Bay window at least 2 ft deep and 5 ft long. For Townhouses this standard is 2 ft deep by 4 ft wide.
 - p. Attached garage width, as measured between the inside of the garage door frame, of 35% or less of the length of the street-facing façade.
 - q. For Townhouses, balconies and bay windows may encroach into a required setback area.
5. Transitional space. Ground floor dwellings which have their entry access from the street or a shared open space (e.g. a common courtyard) must include an area of transition between the public realm of the right-of-way (or

tract or easement). The standards below apply when the private dwelling entry access door is within 10 feet of the street-facing property line; within the front yard setback, or within 10 feet of a shared open space common tract. The transitional space between the public realm and the entry door may be either vertical or horizontal, as described below.

- a. A vertical transition must be an uncovered flight of stairs that leads to the front door or front porch of the dwelling. The stairs must rise at least 3 ft, and not more than 8 ft, from grade. The flight of stairs may encroach into the required front yard, and the bottom step must be at least 4 ft from the front lot line.
 - b. A horizontal transition shall be a covered porch with a depth of at least 6 ft. The porch may encroach into the required front yard, but it must be at least 4 ft from the front lot line.
- 6. Pedestrian circulation. The on-site pedestrian circulation system must include the following:
 - a. Continuous connections between the primary buildings, streets abutting the site, ground level entrances, common buildings, common open space, and vehicle and bicycle parking areas.
 - b. At least 1 pedestrian connection to an abutting street frontage for each 200 linear ft of street frontage.
 - c. Pedestrian walkways must be separated from vehicle parking and maneuvering areas by physical barriers such as planter strips, raised curbs, or bollards.
 - d. Walkways must be constructed with a hard surface material, must be permeable for stormwater, and must be no less than 3 ft wide. If adjacent to a parking area where vehicles will overhang the walkway, a 7-ft-wide walkway must be provided. The walkways must be separated from parking areas and internal driveways using curbing, landscaping, or distinctive paving materials.
- 7. Off-street parking.
 - a. Off-street parking may be arranged in clusters, subject to the following standards:
 - i. Cottage cluster projects with fewer than 16 cottages are permitted parking clusters of not more than five (5) contiguous spaces.




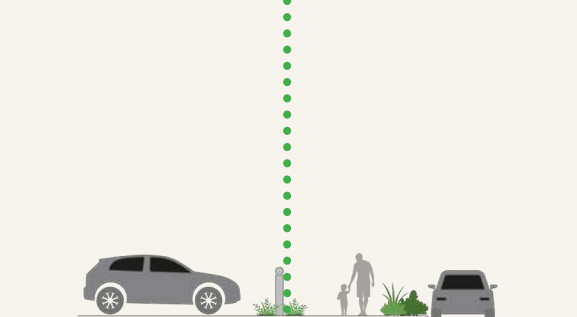
- ii. Cottage cluster projects with 16 cottages or more are permitted parking clusters of not more than eight (8) contiguous spaces.
 - iii. Parking clusters must be separated from other spaces by at least four (4) feet of landscaping.
- b. Clustered parking areas may be covered.
- c. Off-street parking spaces and vehicle maneuvering areas must not be located:
 - i. Within of 20 feet from any street property line, except alley property lines;
 - ii. Between a street property line and the front façade of cottages located closest to the street property line. This standard does not apply to alleys.
- d. Off-street parking spaces must not be located within 10 feet of any other property line, except alley property lines. Driveways and drive aisles are permitted within 10 feet of other property lines.
- e. Landscaping, fencing, or walls at least three feet tall must separate clustered parking areas and parking structures from common courtyards and public streets.
- f. Garages and carports (whether shared or individual) must not abut common courtyards.
- g. Individual attached garages up to 200 square feet must be exempted from the calculation of maximum building footprint for cottages.
- h. Individual detached garages must not exceed 400 square feet in floor area.
- i. Garage doors for attached and detached individual garages must not exceed 20 feet in width.

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FRONTAGE TYPES

BUILD-TO LINE / FRONTAGE LINE

FRONTAGE CATEGORIES

SHOPFRONT			
FORECOURT			
PORCH-STOOP-TERRACE			
LANDSCAPE FRONTAGE 1, 2, AND 3			

URBAN Frontages

URBAN RESIDENTIAL Frontages

RETAIL-READY

LANDSCAPE Frontages

EXAMPLES



Shopfront frontage is characterized by a façade that is aligned close to the frontage line with the building entrance at sidewalk grade.



Forecourt frontage is characterized by portions of the façade close to the frontage line alternated by portions of the façade which are set back.

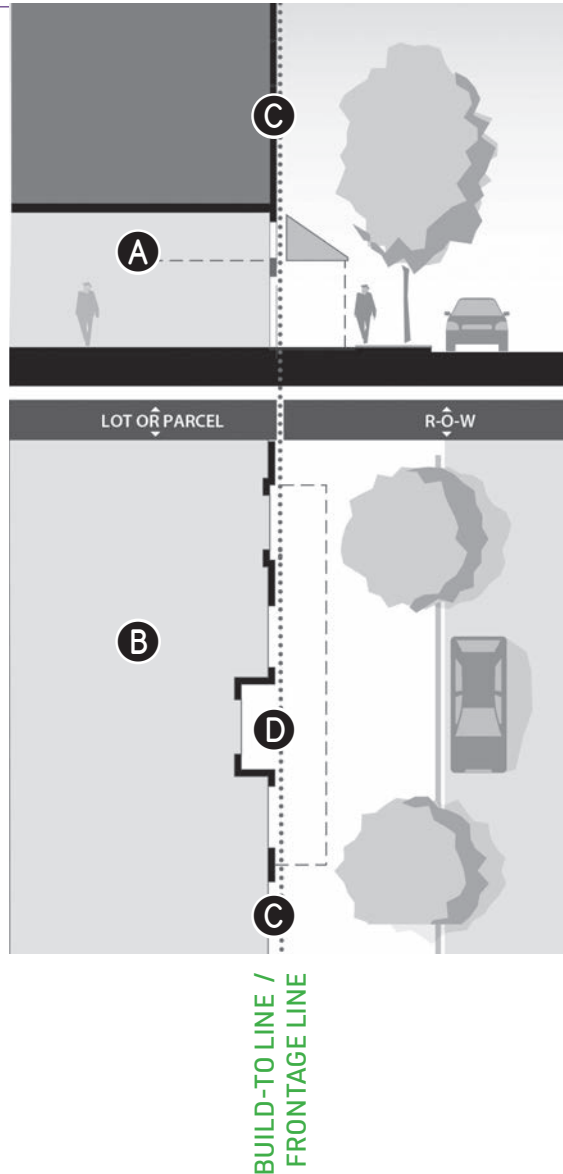


Porch-stoop-terrace frontage is characterized by a façade that is aligned close to the frontage line with the first story set back from the frontage line with a porch, a stoop, or a terrace.



Landscape frontage 2 is characterized by a low wall, fence, and/or vertical landscaping such as a hedge that is aligned with the frontage line.

FRONTAGE TYPES



URBAN FRONTAGE – SHOPFRONT

The Shopfront Frontage is characterized by a façade that is built up to the Build To Line or Frontage Line which is typically the front lot line.

The building entrance shall be at sidewalk grade. The Shopfront Frontage shall have substantial glazing on the ground floor. Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

C Building Setback from Build-to Line (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.

Encroachments such as canopies, awnings, or bay windows are allowed to encroach beyond the Build To Line or Frontage Line (or into the Right of Way) by a maximum of 3 feet.



*Retail-ready
shopfronts*

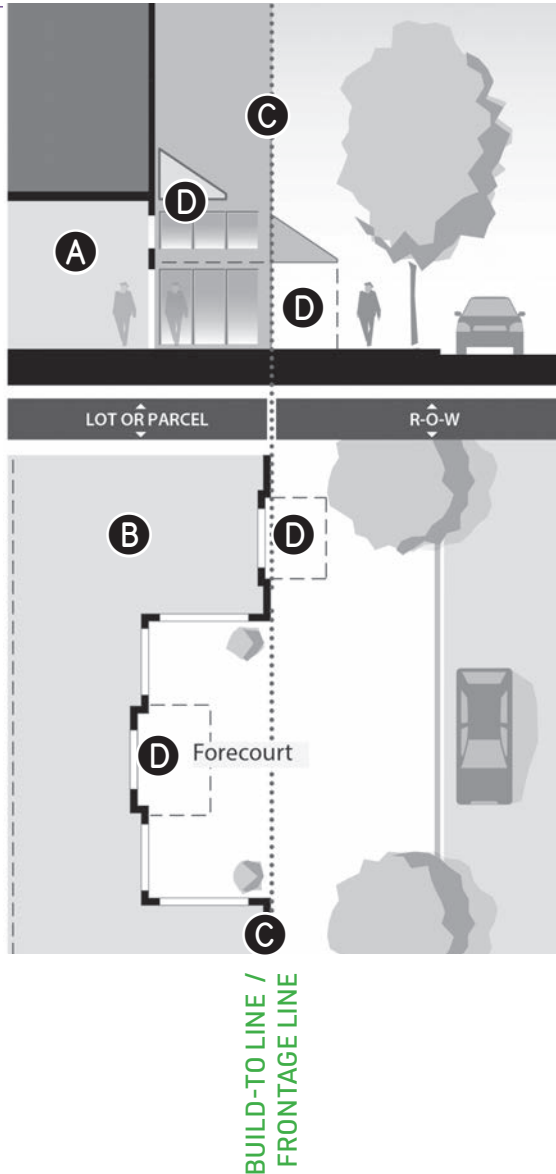


*Bay window and
awning encroachments
into the Right of Way*



Recessed entry

FRONTAGE TYPES



URBAN FRONTAGE – FORECOURT

Forecourt Frontage is created by recessing a portion of the façade behind the Build To Line or Frontage Line. Forecourt Frontage shall be used in conjunction with the Shopfront Frontage. Forecourt Frontage is appropriate for commercial or residential uses, outdoor seating, and hardscaped plaza and/or landscaped gardens.

When marked on the Regulating Plan as “Retail-ready Frontage Requirement,” additional standards must be met to ensure that the ground floor of the building can accommodate retail and/or restaurant uses.

A “Retail ready” minimum ground floor height (feet) 18

B “Retail ready” minimum ground floor depth (feet) 20

C Building Setback from Build-to Line for Shopfront Frontage (feet) 0

Retail ready ground floor shall be constructed to meet commercial building standards.

C Each dwelling unit which is accessed from the forecourt or the sidewalk shall comply with either the vertical or horizontal standard.

Vertical separation –
Distance from ground:
Minimum 18 inches;
maximum 3 feet;
Horizontal separation –
Distance from Build To
Line: Minimum 3 feet;
maximum 15 feet

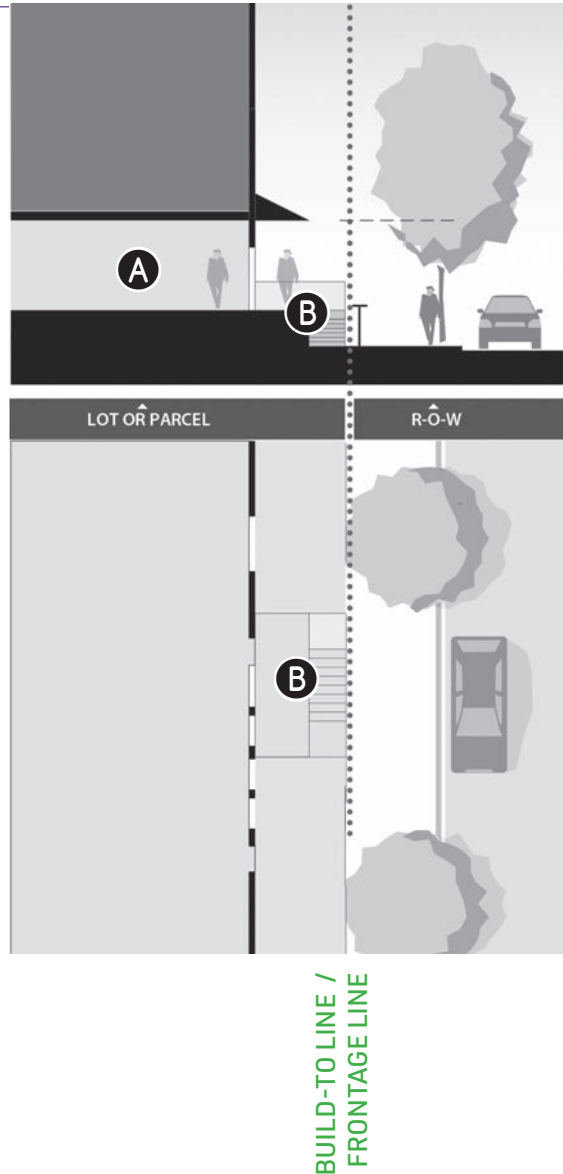
D Building entries shall provide a canopy or awning and/or be recessed behind the front building façade.



Forecourts shared by multiple retail establishments and restaurants



FRONTAGE TYPES



URBAN FRONTAGE – RESIDENTIAL PORCH-STOOP-TERRACE

Urban Residential Building Frontage is characterized by a façade which is set behind the Build To Line and a building entry threshold, such as a porch or terrace, set between the building and the Build To Line or Frontage Line. The threshold may be at the pedestrian sidewalk level, elevated above it, or sunken below it. The residential building entry is accessed from the threshold. Landscaping may be provided in the setback area between the building and the sidewalk. A Porch-Stoop-Terrace Frontage is appropriate for residential uses and service commercial or office uses.

A Minimum ground floor height (feet)

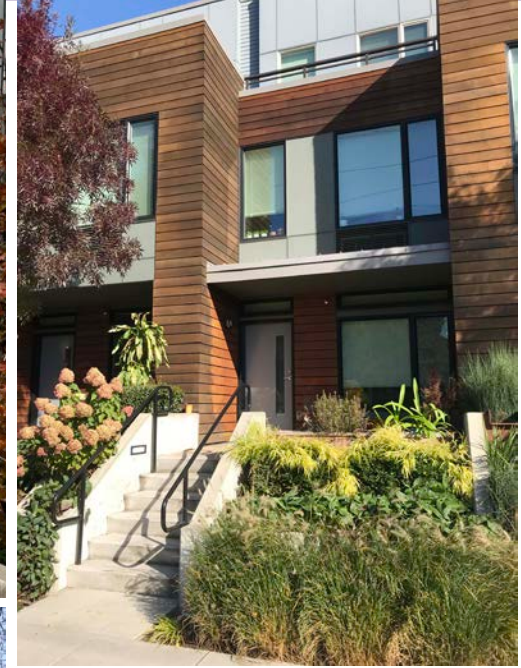
9

B Each dwelling unit which is accessed from the sidewalk shall comply with either the vertical or horizontal standard.

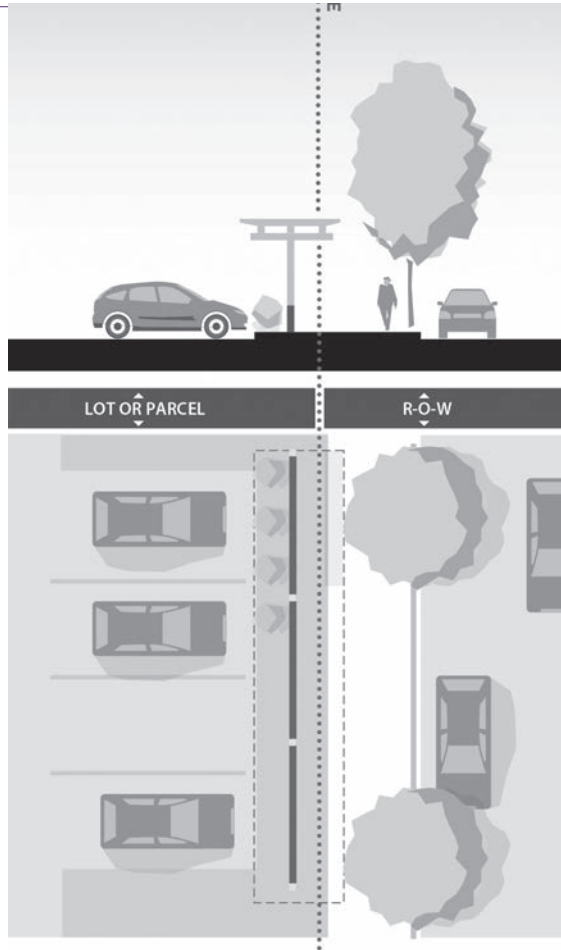
Vertical separation – Distance from ground: Minimum 18 inches; maximum 3 feet;
Horizontal separation – Distance from Build To Line: Minimum 3 feet; maximum 15 feet



*Stoop and terrace
entries*



FRONTAGE TYPES



LANDSCAPE FRONTAGE 1 – LOW WALL AND TRELLIS

A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet

5

B Height of Structure or Planting, feet

The underside of the Trellis portion of a Low Wall and Trellis shall be a minimum of 8 feet above grade and a maximum of 14 feet above grade.

The Low Wall portion of a Low Wall and Trellis shall be a minimum of 1.5 feet and a maximum of 3 feet and have a minimum depth of 1.5 feet.

C Materials

The Trellis shall be heavy timber or steel (or a similar material) and shall consist of open structure with no decking or awning material.

The Trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center.

The Low Wall shall be wood, masonry, and/or concrete.

D Openings

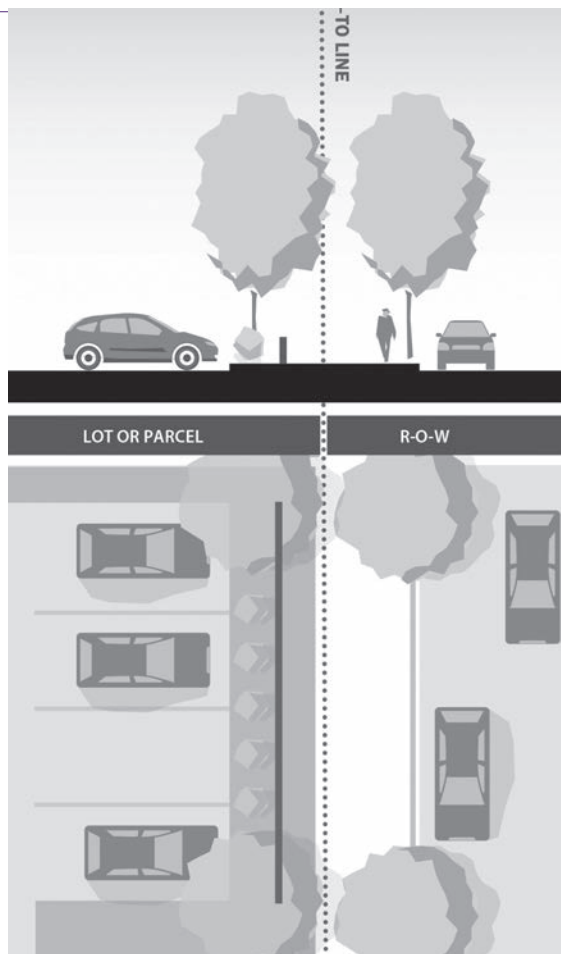
Openings in the Low Wall and Trellis are allowed for pedestrian pathways, sidewalks, plazas, and driveways.

E Surface Parking Setback

Surface Parking shall be set back a minimum of 3 feet from the Low Wall and Trellis.

F Ground Cover and Planting

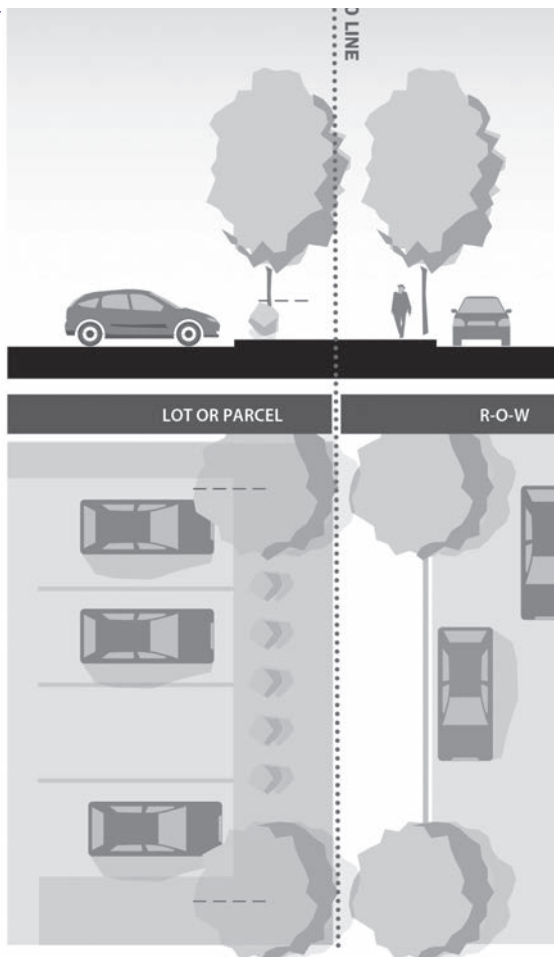
The area between the Build-to Line and the Trellis shall be hardscaped with either masonry pavers or stamped concrete. The setback between the Low Wall and surface parking shall be planted with low shrubs, groundcover, and climbing plants.



LANDSCAPE FRONTAGE 2 - URBAN FENCE OR WALL

- | | |
|---|---|
| A Vertical Landscaping or Structure Setback from Build-to Line, maximum feet | 5 |
| B Height of Structure or Planting, feet | The fence or the wall shall be at least 2 feet high and no more than 3 feet high. |
| C Materials | Walls shall be wood masonry, and/or concrete; fences shall be made of wrought iron, steel, or a similar material (but not chain-link) and must be dark in color. Fences may be no more than 50% sight obscuring. |
| D Openings | Openings in the Urban Fence or Wall are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E Surface Parking Setback | The surface parking area shall be set back, at a minimum, an additional 5 feet to provide room for required landscaping and stormwater infiltration and/or retention. |
| F Ground Cover and Planting | In addition to the required fence or wall, trees and shrubs shall be provided. One large tree is required every 30 linear feet minimum. The shrubs shall be at least as high as the wall or fence, and shall be no more than 6 feet high. |

FRONTAGE TYPES



LANDSCAPED FRONTAGE 3 - LOW HEDGE

- | | | |
|----------|--|---|
| A | Vertical Landscaping or Structure Setback from Build-to Line, minimum feet | 5 |
| B | Height of Structure or Planting, feet | The shrubs shall be a minimum of 3 feet high. If a low wall is provided in place of shrubs it shall be a minimum of 3 feet high. |
| C | Materials | The surface parking area shall be screened with a continuous row of hedges or shrubs immediately adjacent to the parking area, except where there is a driveway. Shrubs must be mostly opaque year round. A low wall may be substituted for the shrubs but the trees and groundcover plants are still required. |
| D | Openings | Openings in the Landscape Frontage are allowed for pedestrian pathways, sidewalks, plazas, and driveways. |
| E | Surface Parking Setback | 10 feet minimum |
| F | Ground Cover and Planting | In addition to the required shrubs, one large tree is required every 30 linear feet. The shrubs/hedge shall be interrupted with a gap of up to 2 feet wide in order to accommodate trees. |

FRONTAGE TYPES





07

References

Traditional Downtown/CBD

Table 3-11 provides design guidance for the respective design elements for ODOT roadways through the Traditional Downtown/CBD context. With this design approach, the goal is to design roadways in the Traditional Downtown/CBD context for a target speed of 20-25 mph. Figure 3-8 illustrates various cross section scenarios for how the design elements within this type of context may be arranged.

Table 3-11: Design Element Recommendations for Traditional Downtown/CBD

	Design Element	Guidance
Pedestrian Realm	Frontage Zone	4' to 2'
	Pedestrian Zone	10' to 8'
	Buffer Zone	6' to 0'
	Curb/Gutter ¹	2' to 0.5'
Transition Realm ⁶	Separated Bicycle Lane (Curb Constrained Facility) ²	8' to 7'
	On-Street Bicycle Lane (not including Buffer) ²	6' to 5'
	Bicycle/Street Buffer ²	3' to 2'
	Right Side Shoulder (if travel lane directly adjacent to curb) ^{3,5}	2' to 0'
	On-Street Parking	7' to 8'
Travelway Realm ⁵	Travel Lane ^{4,5}	11'
	Right Turn Lane (including Sny Distances)	11' to 12'
	Left Turn Lane ⁴	11'
	Left Side / Right Side Sny Distance	1' to 0'
	Two-Way-Left-Turn Lane	11' to 12'
	Raised Median - No Turn Lane (including Sny Distances)	8' to 11'
	Left-Turn Lane with Raised Curb Median/separator (includes 16" separator & Sny Distances)	12' to 14'

¹ Where curb and gutter is used and on-street parking is provided or travel lane is directly adjacent to curb, gutter pan should be included in shoulder/sny or on-street parking measurement. Gutter pan should be included in travel lane, bicycle lane or turn lane measurements only where a smooth transition from gutter pan to roadway surface is provided.

² Refer to Bicycle Facility Selection process (Section 3.2.2) to determine appropriate bicycle facility type. Consider raised bicycle lanes where appropriate. 5-foot on-street bicycle lane is allowed only with a street buffer. When a raised buffer is used to protect the bicycle lane, the width should be 6' if parking is adjacent or if signs or other features are anticipated.

³ Overall shoulder width depends on other section elements. Elimination of shoulder width/lateral offsets should only be considered in constrained locations and needs to be balanced with all cross-section and drainage needs. If the travel lane is next to a curb with a gutter (e.g., a 2-foot curb zone), the gutter typically serves as the right-side shoulder. A wider shoulder may be needed to accommodate drainage based on hydrological analysis or other specific needs.

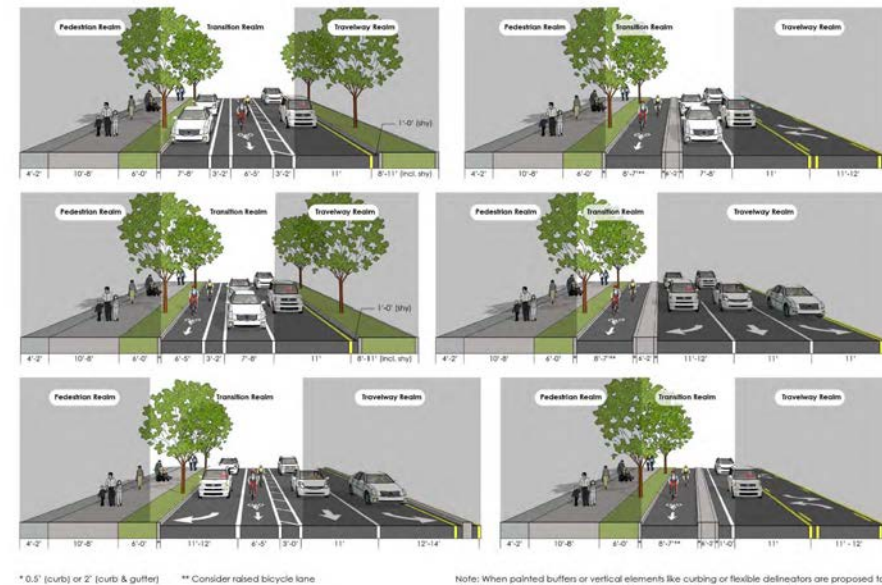
⁴ 11-foot lane width preferred to 12-foot lane; 10-foot lane width requires design approval from the State Roadway Engineer. On freight or transit-oriented streets, a 10-foot travel lane is generally not appropriate without a buffer zone or shoulder.

⁵ On Reduction Review Routes, comply with ODOT Freight Mobility Policies, ORS 366.215 and OAR 731-012. Element dimensions may need to be modified.

⁶ When painted buffers or vertical elements like curbing or flexible delineators are proposed to provide separation in a bicycle facility design, evaluate long-term maintenance needs and provide a solution to identified problems.

ODOT Urban Blueprint

Figure 3-8: Example Cross Section Options for Traditional Downtown/CBD. See Table 3-11 for additional information.



Urban Mix

Table 3-12 provides design guidance for the respective design elements for ODOT roadways through the Urban Mix context. With this design approach, the goal is to design roadways for a target speed of 25-30 mph. Figure 3-9 illustrates various cross section scenarios for how the design elements within this type of context may be arranged.

Table 3-12: Design Element Recommendations for Urban Mix

	Design Element	Guidance
Pedestrian Realm	Frontage Zone	1'
	Pedestrian Zone ⁷	8' to 5'
	Buffer Zone	6' to 0'
	Curb/Gutter ¹	2' to 0.5'
Transition Realm ⁶	Separated Bicycle Lane (Curb Constrained Facility) ²	8' to 7'
	On-Street Bicycle Lane (not including Buffer) ²	6' to 5'
	Bicycle/Street Buffer (preferred for On-Street Lane) ²	4' to 2'
	Right Side Shoulder (if travel lane directly adjacent to curb) ^{3,5}	2' to 0'
	On-Street Parking	8'
Travelway Realm ⁵	Travel Lane ^{4,5}	11' to 12'
	Right Turn Lane (including Shy Distances)	11' to 12'
	Left Turn Lane ⁴	11' to 12'
	Left Side / Right Side Shy Distance	1' to 0'
	Two-Way Left-Turn Lane	11' to 12'
	Raised Median – No Turn Lane (including Shy Distances)	8' to 11'
	Left-Turn Lane with Raised Curb Median/Separator (including 16" separator & Shy Distances)	12' to 14'

¹ Where curb and gutter is used and on-street parking is provided or travel lane is directly adjacent to curb, gutter pan should be included in shoulder/shy or on-street parking measurement. Gutter pan should be included in travel lane, bicycle lane or turn lane measurements only where a smooth transition from gutter pan to roadway surface is provided.

² Refer to Bicycle Facility Selection process (Section 3.2.2) to determine appropriate bicycle facility type. Consider raised bicycle lanes where appropriate. 5-foot on-street bicycle lane is allowed only with a street buffer. When a raised buffer is used to protect the bicycle lane, the width should be 6' if parking is adjacent or if signs or other features are anticipated.

³ Overall shoulder width depends on other section elements. Elimination of shoulder width/lateral offset should only be considered in constrained locations and needs to be balanced with all cross-section and drainage needs. If the travel lane is next to a curb with a gutter (e.g., a 2-foot curb zone), the gutter typically serves as the right-side shoulder. A wider shoulder may be needed to accommodate drainage based on hydrological analysis or other specific needs.

⁴ 11-foot lane width preferred; 10-foot lane width requires design approval from the State Roadway Engineer. On right- or transition-oriented streets, a 10-foot travel lane is generally not appropriate without a buffer zone or shoulder.

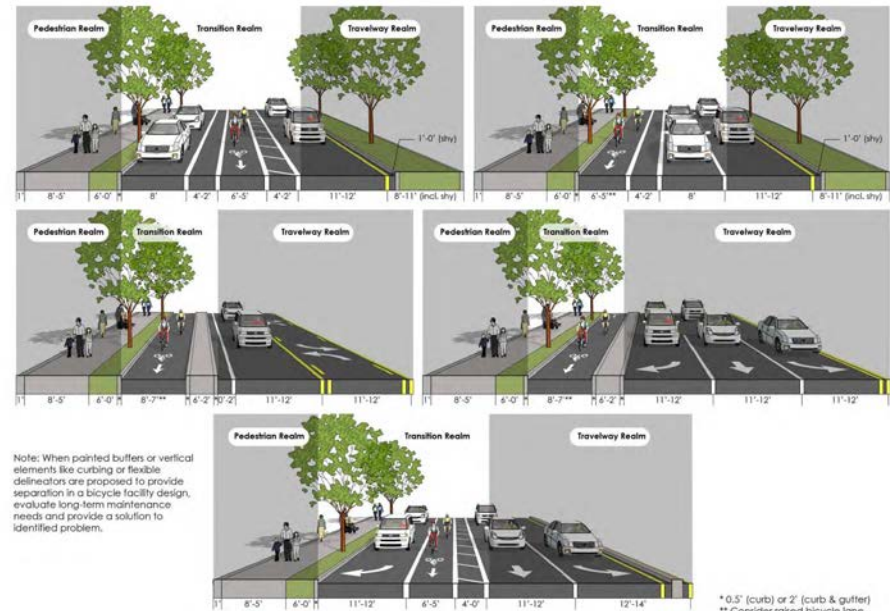
⁵ On Reduction Review Routes, comply with ODOT Freight Mobility Policies, ORS 366.215 and OAR 731-012. Element dimensions may need to be modified.

⁶ When painted buffers or vertical elements like curbing or flexible delineators are proposed to provide separation in a bicycle facility design, evaluate long-term maintenance needs and provide a solution to identified problems.

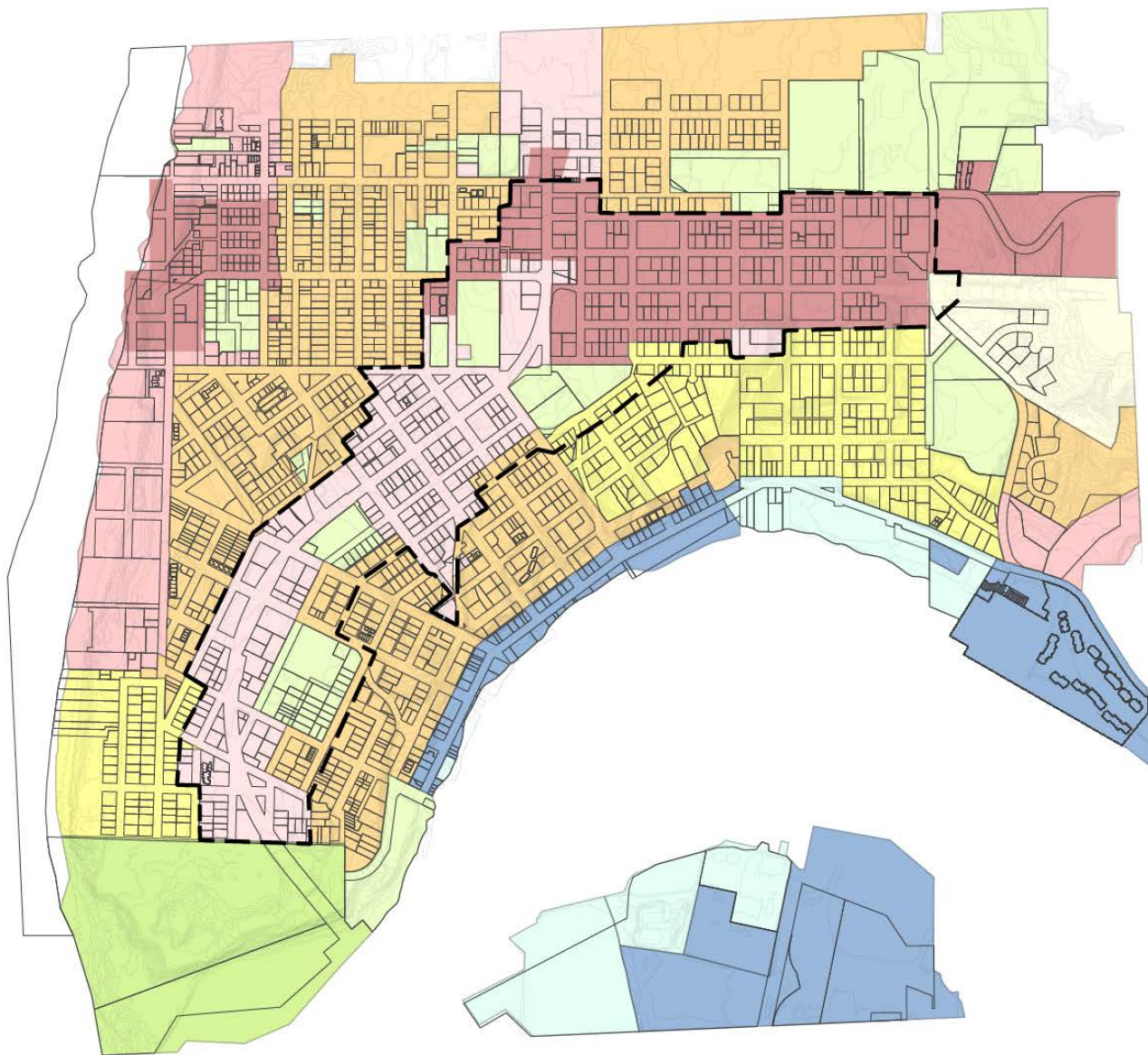
⁷ 5-foot pedestrian zone requires a paved frontage zone and/or a paved buffer zone. Minimum "sidewalk" width is 6-feet.

ODOT Urban Blueprint

Figure 3-9: Example Cross Section Options for Urban Mix. See Table 3-12 for additional information.



Base Zones



Newport Zoning Map

Zoning

C-1	R-1	P-1
C-2	R-2	P-2
C-3	R-3	P-3
I-1	R-4	W-1
I-2		W-2
I-3		

	Tax Lots 2023
	City Center Revitalization Plan Boundary



0 500 1,000 1,500 2,000 ft

Land Uses

CCDRD - LAND USE LISTING

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Uses					
Office/Professional Offices	P	P			C
Retail sales and service					
Sales-oriented, general retail	P	P			
Sales-oriented, general retail	C	P			
Personal services / Beauty and Barber Shops	P	P			C
Entertainment	P	P			
Repair-oriented	P	P			
Major Event Entertainment	C	P			
Vehicle Repair	C	P			
Self-Service Storage	X	P			
Parking Facility	P	P			
Contractors and Industrial Service	X	P			
Manufacturing and Production					

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Custom Creative Work	P	P			
Light Manufacturing	X	C			
Warehouse, Freight Movement, & Distribution	X	P			
Wholesale Sales	X	P			
Waste and Recycling Related	C	C			
Basic Utilities	P	P			
Utility Corridors	C	C			
Community Service	P	P			
Family Child Care Home	P	P			P
Child Care Center	P	P			P
Educational Institutions					
Elementary & Secondary Schools / Public Schools	C	C	P		
College and Universities / Public Colleges or Universities	P	P	P		C
Trade/Vocational Schools/Other	P	P			
Hospitals	C	C			P

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Courts, Jails and Detention Facilities	X	P	P		
Communication Facilities	P	P			
Residences on Floors Other than Street Grade	P	P			
Affordable Housing	P	P			
Transportation Facilities	P	P	P		P
Residential			P		
Single-Family				P	P
Two-Family				P	P
Townhouse				P	P
Single Room Occupancy				P	P
Cottage Cluster				P	P
Multi-family				P	P
Manufactured Homes				P	P
Manufactured Dwelling Park				P	P
Accessory Dwelling Units	P	P		P	P
Accessory Uses				P	P

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Home Occupations				P	P
Community Services / Community Buildings			P		
Parks				P	P
Publicly Owned Recreation Facilities				C	C
Libraries				C	C
Utility Substations				C	C
Public or Private Schools				C	P
Religious Institutions / Places of Worship				C	C
Emergency Shelter				P	P
Residential Care Homes					P
Nursing Homes				X	P
Motels and Hotels				X	C
Rooming and Boarding Houses				P	P
Membership Organizations				X	P
Museums				X	P
Condominiums				P	P

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Hostels				X	C
Recreational Vehicle Parks				X	C
Necessary Public Utilities and Public Service Uses or Structures				C	C
Residential Facility				P	P
Movies Theaters				X	C
Assisted Living Facilities				C	P
Bicycle Shop				X	C
Short-Term Rentals				P	P
Public Open Space			P		
Any Building Erected by a Governmental Entity			P		
Fairgrounds			P		
Public Cemeteries			P		
Water & Wastewater Treatment Plants			P		
Performing Arts Centers			P		
Visual Arts Centers			P		

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Senior Centers			P		
Airport and Accessory Structures			P		
Public Golf Courses			P		
City Halls			P		
County Courthouses			P		
City of County Maintenance Facilities			P		
Publicly Owned Recreational Vehicle Parks			C		
Public Museums			P		
Public Restrooms			P		
Recreation Equipment			P		
Post Office			P		
Parking Lots			P		
Public Hospitals			P		
Water Storage Facilities			P		
Public Libraries					
Fire Stations					

Applicable regulations	C-1	C-3	P-1	R-2	R-4
Police Stations					
Accessory Structures for Any of the Above [Public Uses]					

CHAPTER 14.17 CLEAR VISION AREAS

14.17.010 Purpose

The purpose of this section is to promote safety at intersections and drive access points by reducing obstructions to clear vision at intersections.

14.17.020 Clear Vision Area Defined

A vision clearance area includes the following:

- A. At the intersection of two streets, a triangle formed by the intersection of the curb lines, with each leg of the vision clearance triangle being a minimum of 35 feet in length. Where curbs are absent, the edge of the asphalt or future curb locations shall be used as a guide. The City Engineer may modify this requirement, in writing, upon finding that more or less distance is required (i.e., due to traffic speeds, roadway alignment, etc.).
- B. A portion of a lot subject to a front yard setback as defined in [Section 14.11](#). A clear vision area does not include that portion of a second front yard outside of the area described in subsection (A).

14.17.030 Clear Vision Area Requirements

A clear vision area shall contain no planting, fence, wall, structure, or temporary or permanent obstruction, except for an occasional utility pole or tree, exceeding three feet in height, measured from the top of the curb, or where no curb exists, from the street centerline grade. Trees located within a clear vision area shall have their branches and foliage removed to the height of eight feet above the grade.

14.17.040 Maintenance of Clear Vision Areas

It shall be the duty of the person who owns, possesses, or controls real property or right-of-way adjacent thereto, to maintain a clear vision area in the manner provided in this section.

14.17.050 Exemptions for Buildings

A building erected in compliance with zoning ordinance setbacks is exempt from this section.

14.17.060 Liability

The person owning, in possession of, occupying or having control of any property within the city shall be liable to any person who is injured or otherwise suffers damage by reason of the failure to remove or trim obstructions and vegetation as required by this section. Furthermore, the person shall be liable to the city for any judgment or expense incurred or paid by the city, by reason of the person's failure to satisfy the obligations imposed by this section.

14.17.070 Variances

The requirements of this section shall be subject to the processes and criteria contained in [Section 14.33](#).

(Chapter 14.17 repealed and re-enacted by Ordinance No. 2031, adopted March 5, 2013; and made effective by Ordinance No. 2054 adopted on June 3, 2013; effective June 13, 2013.)

CHAPTER 14.18 SCREENING AND BUFFERING BETWEEN RESIDENTIAL AND NONRESIDENTIAL ZONES

14.18.010 Height Buffer

Non-residential sites of districts abutting or having any portion located adjacent to any residential zone shall have a height limitation beginning at a height of ten feet at the property line abutting the residential zone and increasing at a slope of 1:2 for R-1 property, 1:1 for R-2 property, 2:1 for R-3 property, and 3:1 for R-4 property until intersecting the height limit otherwise established in that district.

14.18.020 Adjacent Yard Buffer

On any portion of a site in a non-residential zone that abuts a residential zone, a minimum interior yard of 10 feet planted and maintained as a landscaped screen shall be required.

14.18.030 Separated Yard Buffer

On any portion of a non-residential site that is opposite from a residential district and separated therefrom by a street, alley, creek, drainage facility, or other open area, a minimum yard of ten feet shall be required. The minimum yard shall be planted and maintained as a landscape screen (excluding areas required for access to the site).

CHAPTER 14.19 LANDSCAPING REQUIREMENT

14.19.010 Purpose

The purpose of this section is to provide for the installation, long-term maintenance and protection of trees, vegetation and other landscape elements within the City of Newport recognizing however, that development often times requires the removal of trees and other plant material. When removal is done, the purpose of this section is to require replacement that is attractive, well placed and enhances the overall appearance of the property and the City as a whole. It is further the purpose of this section to:

- A. Aid in air purification and storm water runoff retardation;
- B. Aid in the reduction of noise and glare;
- C. Provide visual buffers;
- D. Enhance the beauty of the city;
- E. Improve property values;
- F. Reduce erosion; and
- G. To protect and enhance the natural beauty, environment and greenspace within the City of Newport to advance economic development, attract residents and promote tourism.

14.19.020 Definitions

For purposes of this section, the following definitions shall apply. Where no definition is given, the common usage of the word shall be used. If there is a conflict between the definitions contained in this section and the more general definitions contained in the definitions section of this Ordinance, this section shall apply.

- A. Addition. An increase in the gross floor area.
- B. Bayfront. The area of the city defined in the Bayfront Plan section of the City's Comprehensive Plan.

- C. Buffer. The use of landscaping, or the use of landscaping along with berms or fences, that obscure the sight from an abutting property and uses, that at least partially and periodically obstructs view and noise. For purposes of this Section, the buffer does not count toward the required landscaping.

(* Entire section amended by Ordinance No. 1827 (9-7-00).)

14.19.020 – Definitions

- D. City Center. The area of the city defined in Section 14.14.100(C) of this Code.

- E. Development. That which is done on a tax lot or parcel of property under one ownership pursuant to any permit issued by the City of Newport Department of Planning and Community Development.

- F. Gross Floor Area. The total area of a building measured by taking the outside dimensions of the building at each floor level intended for occupancy or storage.

- G. Hanging Basket. A basket of flowers or other plant material that is over a public right-of-way or private property and attached to a building, pole, wall, tree or other attachment. In no case shall a hanging basket be less than eight feet above a sidewalk or other pedestrian way or within two feet of a street or driveway.

- H. Landscaped Area. That area within the boundaries of a given lot or other area authorized for landscaping purposes which is devoted to and consists of landscaping.

- I. Landscaping. Material placed in a landscaped area including but not limited to grass, trees, shrubs, flowers, vines and other groundcover, native plant materials, planters, brick, stone, natural forms, water forms, aggregate and other landscape features, provided, however that the use of brick, stone,

aggregate or other inorganic materials shall not predominate over the use of organic plant material. Landscaping does not include sidewalks, fences, walls, benches or other manufactured materials unless same is incidental to the total area of the landscaped area.

- J. Landscaping Plan. A drawing to scale showing the location, types and density of landscaping.
- K. Maintain or Maintenance. Any activity such as pruning, mulching, mowing, fertilizing, removal and replacement of dead vegetation and other similar acts that promote the life, growth, health or beauty of the landscape vegetation.
- L. Nye Beach. The area of the City defined in the Historic Nye Beach Overlay District section of this Ordinance.
- M. Planter. A decorative container for flowers, bushes, trees and other plant materials including but not limited to window boxes, planter boxes, flower pots and other containers.
- N. Sight obscuring. Landscaping, berms, fences, walls or a combination of all those elements that completely blocks the ability to see through it.
- O. Window or Planter Box. A decorative box, pot, or other container that contains flowers and other plant material that is placed immediately below a window, along a walkway or other location. In no case shall a window or planter box extend more than two feet or 20% of the distance from the building to the street curb into the public right-of-way, whichever is less.

14.19.030 Applicability

The provisions of this ordinance shall apply to all new commercial, industrial, public/institutional, and multi-family development, including additions to existing development or remodels.

(Section 14.19.030 was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

14.19.040 General Requirements

The objective of this section is to encourage the planting and retention of existing trees and other vegetation to improve the appearance of off-street parking areas, yard areas and other vehicular use areas; to protect and preserve the appearance, character, and value of surrounding properties, and thereby promote the general welfare, safety and aesthetic quality of the City of Newport; to establish buffer strips between properties of different land uses in order to reduce the effects of sight and sound and other incompatibilities between abutting land uses; to insure that noise, glare and other distractions within one area does not adversely affect activity within the other area. Prior to the issuance of a building permit, landscaping plans showing compliance with this section are required.

- A. No landscape plan submitted pursuant to this section shall be approved unless it conforms to the requirements of this ordinance.
- B. Landscape plans shall be submitted for all development. Said plans shall include dimensions and distances and clearly delineate the existing and proposed building, parking space, vehicular access and the location, size and description of all landscape areas and materials.

(Section 14.19.040(B) was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

- C. Landscaping shall not obstruct the view at the intersection of two or more streets or alleys; or at the intersection of a street and a driveway.
- D. A guarantee of performance bond or escrow agreement shall be required in an amount to be determined by the Planning Director and approved by the City Attorney as to form to insure satisfactory completion of the landscaping plan as approved if the required landscaping is not installed prior to certificate of occupancy as required by the Building Code.

14.19.050 Landscaping Required for New Development, Exceptions

All new development, except for one and two family residences, shall be required to install landscaping per this section. For purposes of this section, new development shall mean construction upon a vacant lot or a lot that becomes vacant by virtue of the demolition of an existing building. Landscaping shall be provided as follows:

- A. Area. Landscaping shall be ten percent of the total square footage of a lot or parcel.
- B. Location. Landscaping shall be located along a street frontage or frontages.
 - 1. For commercial and industrial zoned lots south of the Yaquina Bay Bridge that abut US 101, landscaping shall include a minimum 15-foot wide landscape buffer.

(Section 14.19.050(B)(1) added by the adoption of Ordinance No. 2196 on November 7, 2022; effective December 6, 2022.)

- C. Exceptions. The right-of-way between a curb and a property line, not counting any sidewalk, driveway or other hard surfaces, may be used and counted toward the required landscaping as long as it has been determined by the Planning Director that the right-of-way is not needed for future street expansion. A developer may also plant a street tree within the sidewalk and it shall count toward meeting landscaping requirements subject to approval by the Planning Director and the City Engineer. A window or planter box may also be used to meet landscaping requirements at a ratio of 1 to 1. If the developer chooses to exercise this option, he or she shall enter into an agreement that the landscaping in the right-of-way is to be maintained as landscaping.
- D. Landscaping and Screening for Parking Lots. The purpose of this subsection is to break up large expanses of parking lots with landscaping. Therefore, all parking areas or each parking bay where a development contains multiple parking areas shall comply with the following provisions:
 - 1. A minimum of 10 percent of the total surface area of all parking areas, as measured around the perimeter of all parking spaces and maneuvering

areas, shall be landscaped. This 10 percent landscaping requirement includes landscaping around the perimeter of parking areas as well as landscaped islands within parking areas. Such landscaping shall consist of canopy trees distributed throughout the parking area. A combination of deciduous and evergreen trees, shrubs, and ground cover plants is required. At a minimum, one tree per 12 parking spaces on average shall be planted over and around the parking area.

2. All parking areas with more than 20 spaces shall provide landscape islands with trees that break up the parking area into rows of not more than 12 contiguous parking spaces. Landscape islands and planters shall have dimensions of not less than 48 square feet of area and no dimension of less than 6 feet, to ensure adequate soil, water, and space for healthy plant growth;
3. All required parking lot landscape areas not otherwise planted with trees must contain a combination of shrubs and groundcover plants so that, within 2 years of planting, not less than 50 percent of that area is covered with living plants; and
4. Wheel stops, curbs, bollards or other physical barriers are required along the edges of all vehicle-maneuvering areas to protect landscaping from being damaged by vehicles. Trees shall be planted not less than 2 feet from any such barrier.
5. Trees planted in tree wells within sidewalks or other paved areas shall be installed with root barriers, consistent with applicable nursery standards.
6. The edges of parking lots shall be screened to minimize vehicle headlights shining into adjacent rights-of-way and residential yards. Parking lots abutting sidewalk or walkway shall be screened using a low-growing hedge or low garden wall to a height of between 3 feet and 4 feet.

7. The provisions of this subsection do not apply to areas for the storage and/or display of vehicles.

(Section 14.19.050(D.) amended by the adoption of Ordinance No. 2199 on August 15, 2022; effective September 14, 2022.)

14.19.060 Landscaping Requirements for Additions and Remodels

For purposes of this section, addition means any development that increases the floor area of a building. Remodel is any work requiring a building permit. For additions and remodels, landscaping shall be provided as follows:

- A. Area. If the subject development after completion complies with the requirements for new development, no additional landscaping is required. If the subject development does not comply with the requirement for new development, landscaping shall be installed so as follows:
 1. For projects with a value of \$80,000 or less, no additional landscaping is required.
 2. For projects with a value of \$80,001 to \$160,000, the amount of landscaping shall be no less than 25% of that required for new development.
 3. For projects with a value of \$160,001 to \$250,000, the amount of landscaping shall be no less than 50% of that required for new development.
 4. For projects with a value of \$250,001 to \$475,000, the amount of landscaping shall be no less than 75% of that required for new development.
 5. For projects with a value greater than \$475,000, the amount of landscaping shall be 100% of that required for new development.

Values shall be based on year 2022 dollars and adjusted on July 1 of each year for inflation. The adjustment shall be based on the latest available U.S. Department of Labor, Bureau of Labor Statistics, Consumer Price Index for All Urban Consumers (CPI-U).

For purposes of this section, the value shall be based on the amount placed on the application for a building permit. If the Building Official determines that the value is below the actual value as calculated by the formulas developed by the State of Oregon Building Codes Division, the value on the permit shall be as determined by the Building Official. If there is a dispute as to the value, the matter shall be referred to the Planning Commission for resolution. The procedure used shall be the same as for a Type I variance contained in [Section 14.33](#) of this Ordinance.

In the case where a second addition or remodel is commenced within one year of the first addition or remodel, the two projects shall be counted as one with regard to determining the above landscaping requirements.

B. Location. Landscaping shall be located along a street frontage or frontages.

1. For commercial and industrial zoned lots south of the Yaquina Bay Bridge that abut US 101, landscaping shall include a minimum 15-foot wide landscape buffer.

C. Exceptions. The right-of-way between a sidewalk and a property line may be used and counted toward the required landscaping as long as it has been determined by the Planning Director that the right-of-way is not needed for future street expansion. If the developer chooses to exercise this option, he or she shall enter into an agreement that the landscaping in the right-of-way is to be maintained as landscaping. In addition, window boxes may be substituted for surface landscaping. The calculation shall be one square foot of window box accounts for three square feet of surface landscaping as required in Subsection A of this Section. A developer may also plant a street tree within the sidewalk and it shall count toward meeting landscaping requirements subject to approval by the Planning Director and the City Engineer.

Development in the Historic Nye Beach Design Review District shall follow the same landscaping requirements as [Subsection 14.19.080](#) (City Center and Bayfront) of Section 14.19 if landscaping requirements are not specified elsewhere. If landscaping is required under a permit issued under the design review design guidelines or design standards, then the permit requirements shall be the applicable landscaping requirements. If the permit requirements specify landscaping requirements that are to be implemented in conjunction with, or in addition to, the landscape requirements of this section, then the landscaping requirements of the permit shall be implemented in conjunction with, or in addition to, the requirements of landscaping specified in [Subsection 14.19.080](#) (City Center and Bayfront) of Section 14.19.

** Amended by Ordinance No. 1865 (12-1-03).*

14.19.080 City Center and Bayfront

Because the City Center and Bayfront areas were platted and built on very small lots and many of the existing buildings are located on or near the property lines, a strict area landscaping requirement is difficult to obtain and places an undue burden on the property owner. Those areas shall therefore be subject to this section rather than [Sections 14.19.050](#) and [14.19.060](#) of this ordinance.

- A. New Development. The requirement for new development, defined as building on a vacant lot, shall be 10% of the lot area. In lieu of the 10%, hanging baskets or window/planter boxes may be substituted for surface landscaping, or any combination thereof. The calculation for square footage may be up to one square foot of hanging basket, planter box or window box for every three feet of otherwise required landscaping.
- B. Additions. Landscaping shall be required at a rate of 10% of the area of the addition. In lieu of the 10%, hanging baskets or window/planter boxes may be substituted for surface landscaping, or any combination thereof. The calculation for square footage may be up to one square foot of hanging basket, planter box or window box for every three feet of otherwise required landscaping.

- C. Remodels. Landscaping shall be required per [Section 14.19.060](#) except that in lieu of providing surface landscaping, window/planter boxes or hanging baskets may be substituted at a rate of one square foot of window/planter box or hanging basket for every ten square feet otherwise required.

14.19.090 Maintenance of Required Landscaping

Landscaping required by this section, whether existing prior to January 1, 1999 or not, shall be reasonably maintained based on the time of year and kept free of weeds and garbage. Failure to maintain required landscaping may be found to be a violation and subject to penalties contained in [Section 14.54](#) of this Code.

14.19.100 Variances

Variances to the requirements of this section shall be subject to the processes and criteria contained in [Section 14.33](#), Adjustments and Variances, and [Section 14.52](#), Procedural Requirements.* As a condition of approval, the Planning Commission may require a bond to assure satisfactory completion of the required landscaping. The Planning Commission may also approve, in lieu of providing a strict landscaping area, window or planter boxes in numbers and size to comply with the intent of this section or a reduction of up to 25% of the required landscaping when the Commission finds that the architectural character of the building is of such quality to justify the reduction. The Commission may also waive up to 25% of the area requirement if the developer puts in an automatic sprinkling system to water the landscaping. The required parking may be reduced up to 10% of the number ordinarily required by this Code if the parking spaces lost is put into landscaping. The site plan prepared by a registered surveyor as required by [Sections 14.33.040](#) is not required for a variance under this Section. If there is a neighborhood design review process, that process supersedes the requirements in this section and, if the design review committee finds that the landscaping is consistent with their review, supersedes the need for a variance otherwise required by this Section.

*(*Amended by Ordinance No. 1989 (1-1-10).)*

CHAPTER 14.30 DESIGN REVIEW STANDARDS

14.30.010 Purpose

Design review districts may be adopted by the City of Newport in accordance with applicable procedures to ensure the continued livability of the community by implementing standards of design for both areas of new development and areas of redevelopment. Design review is an important exercise of the power of the City to regulate for the general welfare by focusing on how the built environment shapes the character of the community.

The Newport Comprehensive Plan identifies six potential urban design districts within the Newport Peninsula including the City Center District (and Highway 101 corridor), Waterfront District, Nye Beach District, Upland Residential District, East Olive District, and the Oceanfront Lodging/Residential District. Additionally, neighborhood plans may be adopted for other areas of Newport that include as an objective the implementation of design review to maintain and/or provide a flexible approach to development by offering two methods of design review from which an applicant can choose. One method of design review is under clear and objective design standards and procedures to allow development that is consistent with the standards to occur with certainty in a timely and cost effective manner. A second alternative method of design review is review under design guidelines, which are a more flexible process for proposals that are creative/innovative and meet the identified guidelines of the applicable design review district.

It is further the purpose of these standards to:

- A. Preserve the beautiful natural setting and the orientation of development and public improvements in order to strengthen their relationship to that setting.

- B. Enhance new and redeveloping architectural and landscape resources to preserve and strengthen the historic, scenic and/or identified neighborhood character and function of each setting.
- C. Improve the vehicular and pedestrian networks in order to improve safety, efficiency, continuity, and relationships connecting Newport neighborhoods.
- D. Strengthen Newport's economic vitality by improving its desirability through improved appearance, function, and efficiency.
- E. Improve the built environment in order to strengthen the visual appearance and attractiveness of developed areas.
- F. Implement the goals and objectives of the adopted neighborhood plans.

14.30.020 Design Review Districts: Overlay Zones Established

The following:

- A. Historic Nye Beach Design Review District. The Historic Nye Beach Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters HNBO and is the area described as follows:

Beginning at the northeasterly corner of SW Hubert Street and SW 2nd Street; thence westerly along the north line of SW 2nd Street to the west line of SW Dolphin Street, said point also being the southeast corner of Lot 1, Block B, Barlow Blocks Addition to the City of Newport; thence north along the west line of SW Dolphin Street to 10 feet beyond the north line of Lot 7, said Barlow Blocks Addition; thence westerly, 10 feet north of and parallel with said north line of Lot 7 to the Pacific Ocean; thence northerly along the Pacific Ocean to the south line of NW 12th Street; thence east along the south line of NW 12th Street to the east line of an alley between NW Spring Street and NW Hubert Street; thence south along the east line of said alley way to

the north line of NW 10th Street; thence southwesterly to the southwest corner of the intersection of NW 10th Street and NW Brook Street; thence south along the west line of NW Brook Street to the south line of NW 8th Street; thence east along the south line of NW 8th Street to the west line of NW Hurbert Street; thence south along the west line of NW Hurbert Street to the north line of NW 6th Street; thence east to the northeast intersection of NW 6th Street and NW Hurbert Street; thence south along the east line of NW Hurbert Street and SW Hurbert Street to the north line of SW 2nd Street and the point of beginning.

B. City Center Design Review District. The City Center Design Review District Overlay Zone shall be indicated on the Zoning Map of the City of Newport with the letters CCDRD and is the area described as follows:

The City Center Design Review District is US Highway 101 and adjacent properties between the north end of the Yaquina Bay Bridge and US Highway 20, and US Highway 20 and adjacent properties between US Highway 101 and the eastern city limits.

14.30.030 Adoption of Design Review: Guidelines and Standards

The document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015, is hereby adopted by reference and made a part hereof. The guidelines and standards contained therein shall be the guidelines and standards applicable to the Historic Nye Beach Design Review District.

The document entitled “City Center Design Review District Form Based Code ” dated [September 2025], is hereby adopted by reference and made a part hereof. The Regulating Plan, Public Realm Standards, and Development and Design Standards, contained therein shall be the standards applicable to the City Center Design Review District.

14.30.040 Design Review Required

The following development activities in an established design review district are required to obtain a design review permit under the design standards in an identified design review district or, in the alternative, to apply for a design review permit and to obtain approval under the design guidelines for that design review district:

- A. New construction, substantial improvement, or relocation of one or more dwelling units.
- B. New construction, substantial improvement, or relocation of a commercial or public/institutional building.
- C. New construction, substantial improvement, or relocation of a residential accessory structure that contains more than 200 square feet of gross floor area and is not more than 10 feet in height.
- D. New construction, substantial improvement, or relocation of a commercial accessory structure that contains more than 120 square feet of gross floor area.
- E. An addition that increases the footprint of an existing building by more than 1,000 square feet.

14.30.050 Exemptions

The following activities are exempt from the provisions of this chapter:

- A. Development activity that is subject to the provisions of Newport Municipal Code Chapter 14.23, Historic Buildings and Sites.
- B. Any project for improvement of a structure to comply with existing state or local health, sanitary, or safety code specifications that are solely necessary to assure safe living conditions.
- C. Development that does not involve the construction, substantial improvement, or relocation of a dwelling unit, commercial or public/institutional building, or accessory structure.

- D. Conversion of space within an existing structure into an accessory dwelling unit.

(Section 14.30.050 revised by Ordinance No. 2152, adopted on November 4, 2019; effective December 4, 2019.)

14.30.060 Approval Authority

The following are the initial review authorities for a Design Review application:

- A. Community Development Director. For projects subject only to the design standards specified in ~~the following documents entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015.~~ The approval or denial of a Design Review application by the Community Development Director is a ministerial action performed concurrent with City review of a building permit.
 - a. Entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015. The approval or denial of a Design Review application by the Community Development Director is a ministerial action performed concurrent with City review of a building permit.
 - b. Entitled “City Center Design Review District Form Based Code.”
- B. Planning Commission. For projects that require design review under the design guidelines contained in the document entitled “Newport Design Review: Guidelines and Standards,” dated July 29, 2015, including the following:
 - 1. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that is over 65 feet in length or 35 feet in height; or
 - 2. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure

that does not meet the design standards contained in the document entitled “Newport Design Review: Guidelines and Standards” dated July 29, 2015; or

3. New construction, substantial improvement, or relocation of a dwelling unit; commercial or public/institutional building; or accessory structure that involves a conditional use, a variance, or any other type of land use permit for which a Type III Land Use Action decision process is required, pursuant to [Chapter 14.52](#), Procedural Requirements.

14.30.070 Application Submittal Requirements

- A. For requests that are subject to Community Development Director review for compliance with design standards, an application for Design Review shall consist of the following:
 1. A completed and signed City of Newport Building Permit Application Form.
 2. Building plans that conform to the submittal requirements for a building permit that include a site plan, floor plan, exterior architectural elevations, cross-section drawings, and construction specifications illustrating how the design standards have been met.
 3. A written checklist identifying the design elements used to comply with the design standards.
- B. For requests that are subject to Planning Commission review for compliance with design guidelines, an application for Design Review shall consist of the following:
 1. Submittal requirements for land use actions listed in [Section 14.52.050](#).
 2. Exterior elevations of all buildings on the site as they will appear after development. Such plans shall indicate the material, texture, shape, and other design features of the building(s), including all mechanical devices.

3. A parking and circulation plan illustrating all parking areas, drive isles, stalls, and points of ingress/egress to the site.
4. A landscape plan showing the location, type and variety, size and any other pertinent features of the proposed landscaping and plantings for projects that involve multi-family, commercial, and public/institutional development.

(Section 14.30.070(B)(4) was amended by Ordinance No. 2182, adopted on May 17, 2021: effective June 16, 2021.)

5. A lighting plan identifying the location and type of all permanent area lights, including parking area lighting, along with details of the lighting fixtures that are to be installed.
6. A written set of proposed findings that explain how the project complies with the applicable design guidelines.
7. Any other information the applicant believes is relevant to establishing that the project complies with applicable design guidelines.

C. All plans shall be drawn such that the dimensions can be verified with an engineers or architects scale.

14.30.080 Permitted Uses

In addition to uses permitted outright or conditionally in the underlying zoning district, the following uses are permitted within areas subject to design review.

A. Historic Nye Beach Design Review District.

1. Tourist Commercial (C-2) zoned property.
 - a. Up to five (5) multi-family dwelling units per lot or parcel are permitted outright provided they are located on a floor other than a floor at street grade.
 - b. A single-family residence is permitted outright if located on a floor other than a floor at street grade.

- c. A single-family residence is permitted outright, including the street grade floor, within a dwelling constructed prior to January 1, 2004. Residential use at the street grade is limited to the footprint of the structure as it existed on this date.
- d. Single family, duplex, townhouses, cottage clusters, and multi-family dwelling units, including at the street grade, are permitted outright on property located south of NW 2nd Court and north of NW 6th Street, except for properties situated along the west side of NW Cliff Street.

(Section 14.30.080(A)(1)(d) was amended by Ordinance No. 2182, adopted on May 17, 2021; effective June 16, 2021.)

(Section 14.30.080(A)(1)(d) was amended by Ordinance No. 2165, adopted on June 15, 2020; effective July 15, 2020.)

- a.
- 2. High Density Multi-Family Residential (R-4) zoned property.
 - a. Uses permitted outright in the C-2 zone district that are not specified as a use permitted outright or conditionally in the R-4 zone district, are allowed subject to the issuance of a conditional use permit in accordance with the provisions of Chapter 14.34, Conditional Uses and subject to the limitation that the use not exceed a total of 1,000 square feet of gross floor area. This provision does not preclude an application for a use as a home occupation under [Chapter 14.27](#), Home Occupations.

14.30.090 Prohibited Uses

The following uses are prohibited within areas subject to Design Review.

A. Historic Nye Beach Design Review District

- 1. Any new or expanded outright permitted commercial use in the C-2 zone district that exceeds 2,000 square feet of gross floor area. New or expanded uses in excess of 2,000 square feet of gross floor area may be permitted in accordance with the provisions of [Chapter 14.34](#),

Conditional Uses.

2. Recreational vehicle parks within the Tourist Commercial (C-2) and Public Structures (P-1) zoning districts.

(Section 14.30.090 revised by Ordinance No. 2120, adopted on September 18, 2017; effective October 18, 2017.)

14.30.100 Special Zoning Standards in Design Review Districts

All zoning standards and requirements applicable under Ordinance No. 1308 (as amended) in the subject zoning district shall apply, except that the following additional zoning standards are applicable for the design review district as applicable in the underlying zoning designation and shall be modified for each district as specified.

A. Historic Nye Beach Design Review District:

1. No drive through windows are allowed.
2. Commercial buildings with frontage on NW and SW Coast Street, W Olive Street, NW and SW Cliff Street, NW Beach Drive, and NW Third Street shall be set back from the property line fronting the street no more than 5 feet unless the development provides for a pedestrian oriented amenity (such as a courtyard, patio, or café with outdoor seating), compliance with the setback is precluded by topography or by easement, or a larger setback is authorized by the Planning Commission through the design review process.
3. Required yards and setbacks established in [Chapter 14.11](#) (Required Yards and Setbacks) and [Chapter 14.18](#) (Screening and Buffering between Residential and Non-Residential Zones) shall be reduced by 50%, except for [Section 14.11.030](#), Garage Setback, which is to remain at 20-feet. A setback for a garage that is less than 20-feet may be permitted if it is found by the Planning Commission to be consistent with the Design Review Guidelines pursuant to NMC [14.30.060\(B\)](#).

4. The following adjustments to [Chapter 14.12](#) (Minimum Size) and [Chapter 14.13](#) (Density Limitations, Table “A”) are allowed within the District.
 - a. The minimum lot area within both the R-4 and C-2 zones shall be 3,000 square feet.
 - b. The minimum lot width for the R-4 zone shall be 30 feet.
5. Residential use permitted on C-2 zoned property located south of NW 2nd Court and north of NW 6th Street, except for properties situated along the west side of NW Cliff Street, shall comply with the following additional requirements:
 - a. The maximum residential density is 1,250 square feet per unit.
 - b. The maximum building height is 35 feet.
 - c. The maximum lot coverage in structures is 64%. If the proposed residential use provides at least 1 off-street parking space for each dwelling unit in a below-grade parking structure (for the purposes of this section below-grade is defined to mean that 50% or more of the perimeter of the building is below-grade) located directly below the residential portion of the structure, the maximum lot coverage allowed is 90%.
 - d. The residential use provides at minimum 1 off-street parking space for each dwelling unit.
 - e. At least one residential building per lot is set back from the property line abutting the street no more than 5 feet.

(Section 14.30.100(A)(5) was amended by Ordinance No. 2165, adopted on June 15, 2020; effective July 15, 2020.)

6. The following adjustments to the off-street parking requirements of [Chapter 14.14](#) (Parking, Loading, and Access Requirements) are provided for uses within the District:

- a. Commercial uses shall have the first 1,000 square feet of gross floor area exempted from the off-street parking calculation.
- b. All uses within the District shall be allowed an on-street parking credit that shall reduce the required number of off-street parking spaces by one off-street parking space for every one on-street parking space abutting the property subject to the following limitations:
 - i. Each on-street parking space must be in compliance with the City of Newport standards for on-street parking spaces.
 - ii. Each on-street parking space to be credited must be completely abutting the subject property. Only whole spaces qualify for the on-street parking credit.
- a. On-street parking spaces credited for a specific use may not be used exclusively by that use, but shall be available for general public use at all times. No signs or actions limiting general public use of on-street parking spaces are allowed except as authorized by the City of Newport.

B. City Center Design Review District

- 1. No drive through windows are allowed within the area identifies as the Traditional Downtown Zone as identified in the City Center Design Review District Form Based Code.
- 2. Adjustments to Chapter 14.12 (Minimum Size) and Chapter 14.13 (Density Limitations, Table "A") are allowed within the City Center Design Review District as detailed in the City Center Design Review District Form Based Code, subsection 5, Development and Design Standards.

(Section 14.30.100 revised by Ordinance No. 2120, adopted on September 18, 2017; effective October 18, 2017.)

14.30.110 Modification of a Design Review Permit

A modification of an approved design may be requested of the approving authority for any reason by an applicant. Applications for a modification shall be submitted and processed in the same manner as the original application.

- A. If the requested modification is from an approval issued under design standards, the modification request shall be approved by the Community Development Director if the modification also meets the design standards.
- B. If the modification does not meet the design standards or if the modification is from an approval issued under the design guidelines, the modification shall be processed under the design review process for compliance with the applicable design guidelines. The Commission's authority is limited to a determination of whether or not the proposed modification is consistent with the applicable design review guidelines.

(Chapter 14.30 was revised by Ordinance No. 2084; adopted September 21, 2015; effective October 21, 2015.)

CITY OF NEWPORT

RESOLUTION NO. _____

**A RESOLUTION SETTING FEES AND TERMS FOR USE
OF CERTAIN PUBLIC PARKING AREAS IN NYE BEACH**

WHEREAS, on March 2, 2020, the Newport City Council adopted Ordinance No. 2163, implementing recommendations of a 2018 Parking Study by Lancaster StreetLab, as amended by the City's Parking Advisory Committee; and

WHEREAS, among other things, Ordinance No. 2163 included policy direction to pursue public parking demand management strategies in Nye Beach in high use areas to increase vehicle turnover, reduce congestion, improve public safety, and generate sufficient funds to cover parking maintenance costs; and

WHEREAS, demand management options were considered by the Newport Parking Advisory Committee, and a concept was developed to meter the Nye Beach Turnaround, adjust the time parking limitations of the Visual Arts Center parking areas, and introduce paid parking permits as an option for exceeding parking time limits in areas where parking time limits are currently in place; and

WHEREAS, the parking demand management concept was refined as a result of public outreach that included a neighborhood meeting, direct engagement with affected organizations, and review by the Newport Parking Advisory Committee and Planning Commission; and

WHEREAS, Ordinance No. 2214, adopted provides that fees, time limits, and related terms of use in metered and paid permit areas are to be determined by resolution of the City Council with the proceeds being placed in the City of Newport Parking Fund; and

WHEREAS, this resolution puts in place the fees, time limits, and related terms of use for high demand public parking areas in Nye Beach so that the demand management concept can be implemented.

THE CITY OF NEWPORT RESOLVES AS FOLLOWS:

Section 1. Nye Beach Meter Parking Areas. Metered public parking areas in Nye Beach shall be limited to the Nye Beach Turnaround, as depicted on the attached Exhibit A.

Section 2. Location of Permit/Timed Zones. Areas subject to permit/timed limitations in Nye Beach are those areas currently subject to a 3-hour time limit, 9am-6pm and a portion of the Visual Arts Center parking lot, as illustrated on Exhibit A and more specifically defined below:

- a. Zone E. This permit/timed zone includes those portions of Coast Street between Olive Street and 6th Street, Beach Drive west of Coast Street, the north side of 3rd Street between Cliff Street and Hurbert Street, the south side of 3rd Street between the Pacific Ocean and Brook Street, and that portion of the Visual Arts Center parking area south of the building bordering the Pacific Ocean.

Section 3. Nye Beach Meter Fees and Dates of Operation. Meter pricing shall be \$1.00 an hour. Meters are to operate from 9am to 6pm, seven days a week from May to October. From November to April meters are to operate from 9am to 6pm on Saturdays and Sundays only.

Section 4. Length of Stay. Except as outlined in Section 5, the maximum length of stay in a parking stall located within a permit/timed zone is three hours. The maximum length of stay at the Nye Beach Turnaround and that portion of the Visual Arts Center that is not within a permit/timed zone shall be 16 hours.

Section 5. Permit Fees, Duration, and Availability. The cost, duration, and availability of permits shall be as follows:

- a. Zone E Residential Permit. \$35 annual fee. Residence must be adjacent to a permit/timed parking area. One permit to be made available for each license holder within a household. 72 hour maximum stay.
- b. Zone E Commercial Permit. \$35 a month or \$300 annually, with a 16 hour daily maximum stay, and a maximum number of permits being equivalent to the number of available parking stalls.
- c. Lodging Day Permit. \$10 fee. Available to guests of Nye Beach lodging establishments and short-term rentals. 16 hour daily maximum stay with no limit on the number of daily permits issued.

Section 6. Visual Arts Center Lot. The parking lot south of the Visual Arts Center that provides access to the third floor of that facility is reserved for use by staff and guests of the Visual Arts Center and shall be signed accordingly.

Section 7. Coupons. As provided in NMC 6.20.050, the City Manager shall make available parking meter courtesy permits or coupon codes to volunteers at the Visual Arts Center and Yaquina Art Association, and may make them available for special events, City sponsored promotional events, or circumstances where parking meters malfunction or an error otherwise occurs in the application of the meter program.

Section 8. Periodic Fee Adjustments. The fees set forth herein are to be evaluated and may be adjusted on an annual basis. Inflationary adjustments to the fees shall be made at least

once every five years using the Bureau of Labor Statistics Consumer Price Index for Urban Consumers (CPI-U).

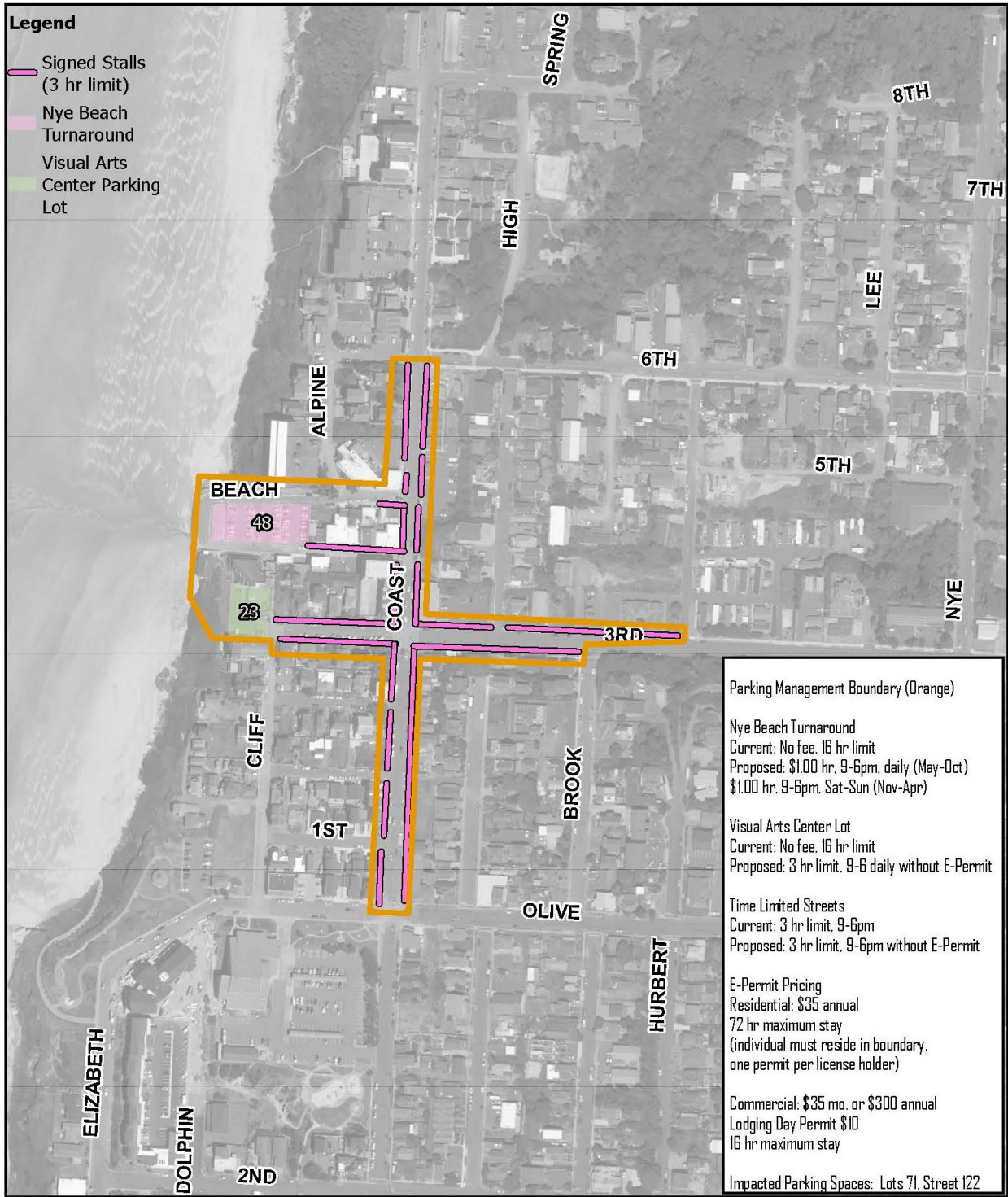
Adopted by the City Council of the City of Newport this ____ day of _____, 2025.

Jan Kaplan, Mayor

ATTEST:

Allie Anderson, City Recorder

DRAFT



NEWPORT City of Newport
Community Development Department
189 SW Coast Highway Phone: 1.541.574.0629
Newport, OR 97365 Fax: 1.541.574.0644

This map is for informational use only and has not been prepared for, nor is it suitable for legal, engineering, or surveying purposes. It includes data from multiple sources. The City of Newport assumes no responsibility for its compilation or use and users of this information are cautioned to verify all information with the City of Newport Community Development Department.

**Draft Nye Beach
Parking Management Plan Concept v3**

Terrain Image Taken July 2024
4-inch, 4-band Digital Orthophotos
GeoTerra, Inc. Eugene, OR

0 150 300 600 Feet



The background image shows the entrance to Nye Beach. A large, arched wooden structure spans the street, with the words "N Y E B E A C H" in blue letters across its top. To the right, there are multi-story wooden buildings with balconies. In the distance, a beach and the ocean are visible under a clear blue sky.

Nye Beach Parking Management Solution

CITY COUNCIL WORK SESSION
APRIL 7, 2025



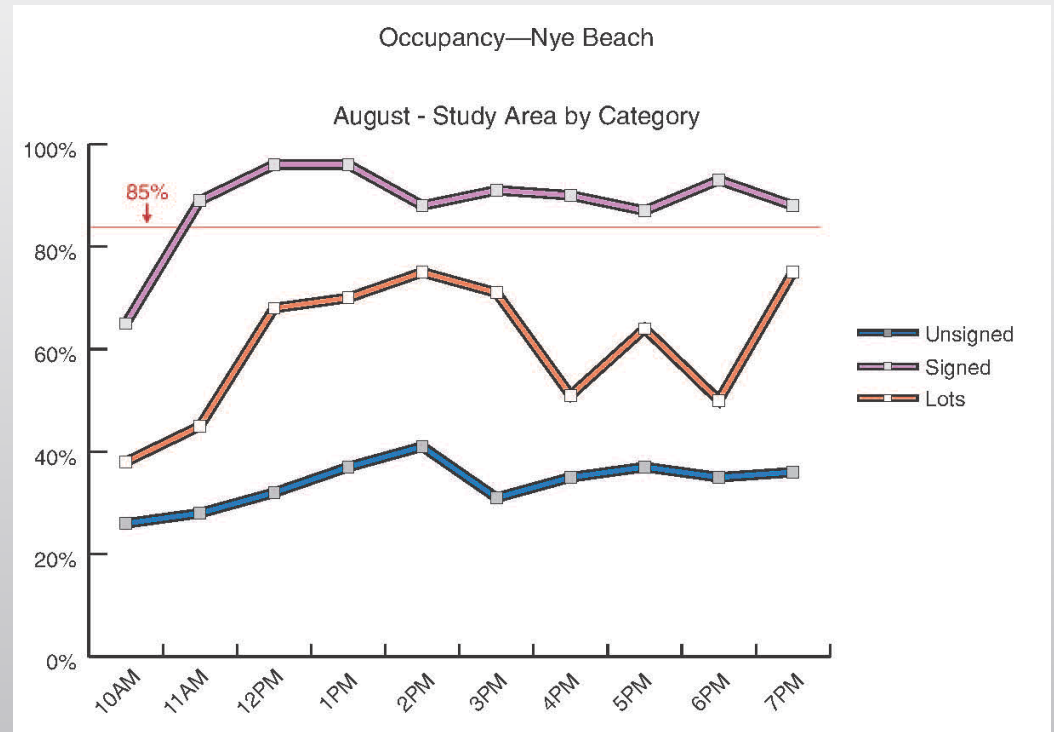
HISTORY OF PARKING MANAGEMENT IN NEWPORT

- New development and redevelopment allowed to pay a fee in lieu of constructing off-street parking
- Businesses in special parking areas pay fee to support public parking and receive exemption from off-street parking requirements for modest development projects
- Seek to change user behavior to free up public parking in high demand areas through metering and permit programs



2018 PARKING STUDY

NYE BEACH STUDY RESULTS



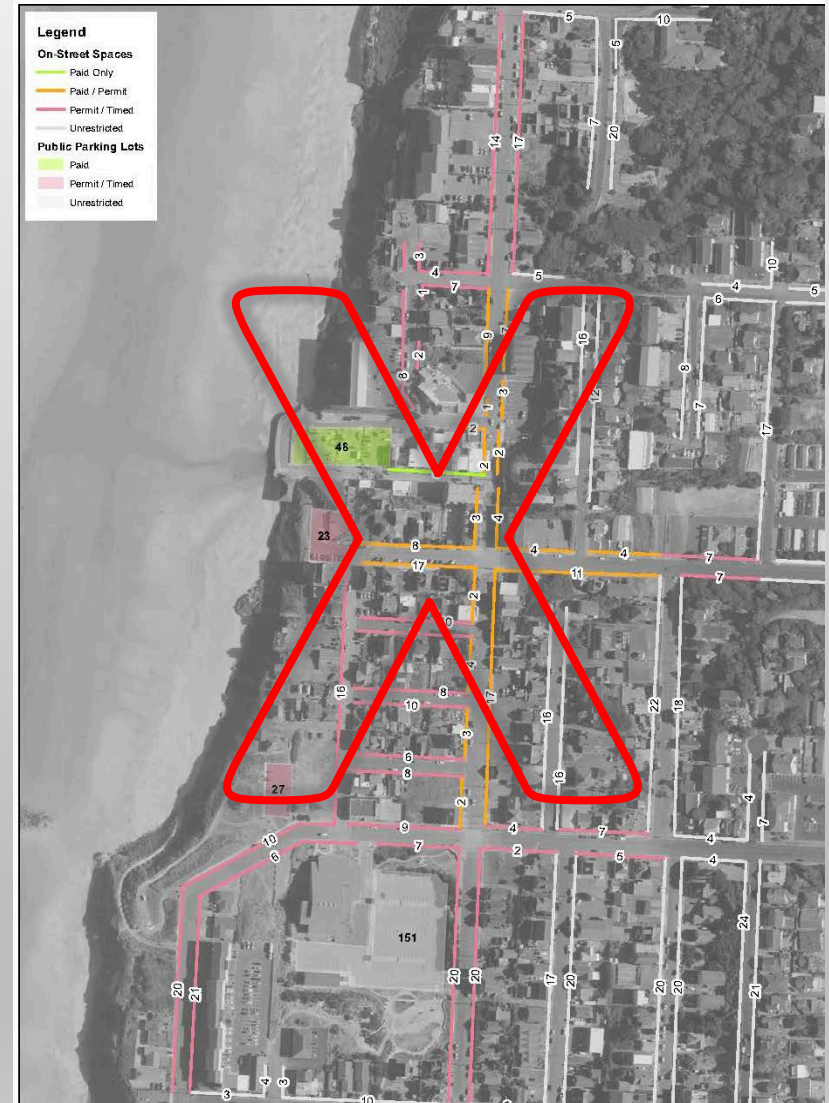
- High instances of illegal parking
- Congestion attributed to vehicles cruising for parking
- Other undesirable behavior from frustrated drivers



2020 COUNCIL ADOPTED NYE BEACH RECOMMENDATION



PARKING STUDY METERING CONCEPT





2020 COUNCIL ADOPTED NYE BEACH RECOMMENDATION

- Implementation Measure 2.1.2: In areas where metering is not implemented, fees from businesses and users should be adjusted to cover anticipated maintenance costs, unless other revenue sources are identified for that purpose (Ord. 2163).



WHAT ARE ANTICIPATED MAINTENANCE COSTS?

Maintenance Schedule

The pavement condition assessment informed the development of a maintenance schedule to identify the level of funding the City should reserve annually to maintain the travel surfaces of the public parking lots (Table 3). Lots that are in good condition can be maintained with a chip seal or slurry seal every 5-10 years, and this is typically done up to three times before the surface is reconstructed. Those in fair condition will need to be rebuilt sooner, and those in poor condition are not candidates for a seal coat, as such treatment is unlikely to extend the useful life of the pavement surface.

Annual estimates were further prepared to account for striping and other ancillary repairs that may be needed, such as drainage, sidewalk, or curb replacement. Placeholders were also provided for administration of a permit parking program and metering, should those elements be implemented. The annual maintenance needs were then broken out by commercial area (Table 4).

Table 3: Parking Lot Surface Maintenance Needs.

Parking Lot	District	Size (sf)	Spaces	Condition	1-5 Years	5-10 Years	10-15 Years	15-20 Years
Angle Street Lot	City Center	30,000	65	Good		Seal \$60,000		Seal \$79,500
City Hall	City Center	57,900	112	Good		Seal \$115,800		Seal \$153,435
Don Davis Park	Nye Beach	9,800	25	Good		Seal \$19,600		Seal \$25,970
Performing Arts Center	Nye Beach	74,800	151	Good		Seal \$149,600		Seal \$198,220
Jump-Off Joe	Nye Beach	6,100	10	Good		Seal \$12,200		Seal \$16,165
Lee Street	Bay Front	11,000	19	Good		Seal \$22,000		Seal \$29,150
Abbey (ROW)	Bay Front	5,800	10	Good		Seal \$11,600		Seal \$15,370
Case (ROW)	Bay Front	3,600	6	Good		Seal \$7,200		Seal \$9,540
9 th & Hurbert	City Center	29,700	48	Fair	Seal \$51,678		Rebuild \$198,099	
US 101 & Hurbert	City Center	9,200	20	Fair	Seal \$16,008		Rebuild \$61,364	
Fall & 13 th	Bay Front	11,800	22	Fair	Seal \$20,532		Rebuild \$78,706	
Hurbert (ROW)	Bay Front	13,400	28	Fair	Seal \$23,316		Rebuild \$89,378	
Canyon Way	Bay Front	23,000	33	Fair	Seal \$40,020		Rebuild \$153,410	
Nye Beach Turnaround	Nye Beach	40,000	45	Poor	Rebuild \$203,616		Seal \$92,920	
Visual Arts Center	Nye Beach	12,900	21	Poor	Rebuild \$65,016		Seal \$29,670	
Fall & Bay	Bay Front	8,600	13	Poor	Rebuild \$43,344		Seal \$19,780	
Abbey Lot	Bay Front	21,200	53	Poor	Rebuild \$106,848		Seal \$48,760	
13 th (ROW)	Bay Front	3,200	7	Poor	Rebuild \$16,128		Seal \$7,360	
Hatfield Lift Station	Bay Front	2,000	5	Poor	Rebuild \$10,080		Seal \$4,600	
					Cost: \$598,586	\$398,000	\$784,047	\$527,350
								Total Cost: \$2,305,983
								Annual \$115,299

Table 4: Annual Maintenance Expenses

Parking District	Lot Resurfacing ¹	Ancillary Repairs ²	Striping	Permit Program ³ (if implemented)	Metering ³ (if implemented)	Total
Bayfront	\$37,850	\$9,450	\$1,850	\$10,000	\$28,800	\$87,950
City Center	\$36,800	\$9,200	\$1,900	Not recommended	Not recommended	\$47,900
Nye Beach	\$30,500	\$7,650	\$1,450	\$10,000	\$13,200	\$62,800

1. Costs from pavement condition assessment prepared as part of parking study. Resurfacing costs proportioned by district with the cost of the Nye Beach Turnaround project being backed out since it has been funded with other resources.

2. Ancillary costs include repairs to drainage system, sidewalks, walls and railing when lots are resurfaced. Assumes 25% of resurfacing cost.

3. Annual maintenance costs are as outlined in the Study (\$500/pay station and \$100/sign).

Parking Study Implementation Approach

Goal B-2 Implement recommendations of the parking plan. (Vision Strategies C1, C3, C8, C9) 2-5 years

Objectives for 2023-2024

C-4(a) Implement the parking management system, including paid parking, permit parking and adjusting time limits for parking in the Bayfront area. Completed

C-4(b) Initiate discussions with Nye Beach businesses and residents regarding appropriate permit and timed parking solutions for the Nye Beach area. Deferred to 2025-26

Objectives for 2025-2026

25-B-2(a) Initiate discussions with Nye Beach businesses and residents regarding appropriate permit and timed parking solutions for the Nye Beach area. Underway

Parking Management Vendor Selection

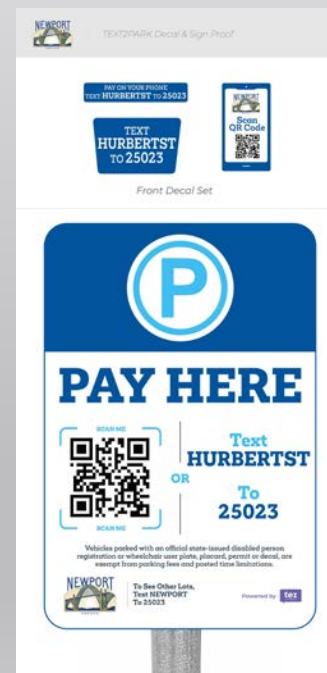
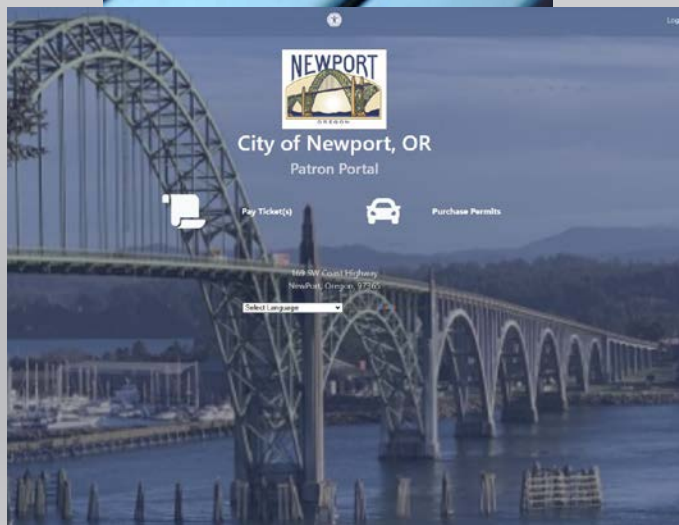
- Parking Advisory Committee Reformed
- Request for Proposals Issued
- Vendor Responses Reviewed and Scored
- Preferred Vendor T2 Systems Selected
- Contract Negotiated and Signed

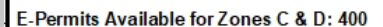




Parking Management Solutions by T2 Systems

- Integrated solar powered wireless coin/card and coupon code pay stations
- Mobile "text to pay" option
- Electronic permit and ticket payment through a linked online portal
- Parking citation solution using license plate recognition technology to improve enforcement capabilities
- Collections support







Bayfront Parking Budget



Bayfront 6-Month Council Report

- Meters: \$250,257
- Permits: \$54,085
- Tickets: \$32,521

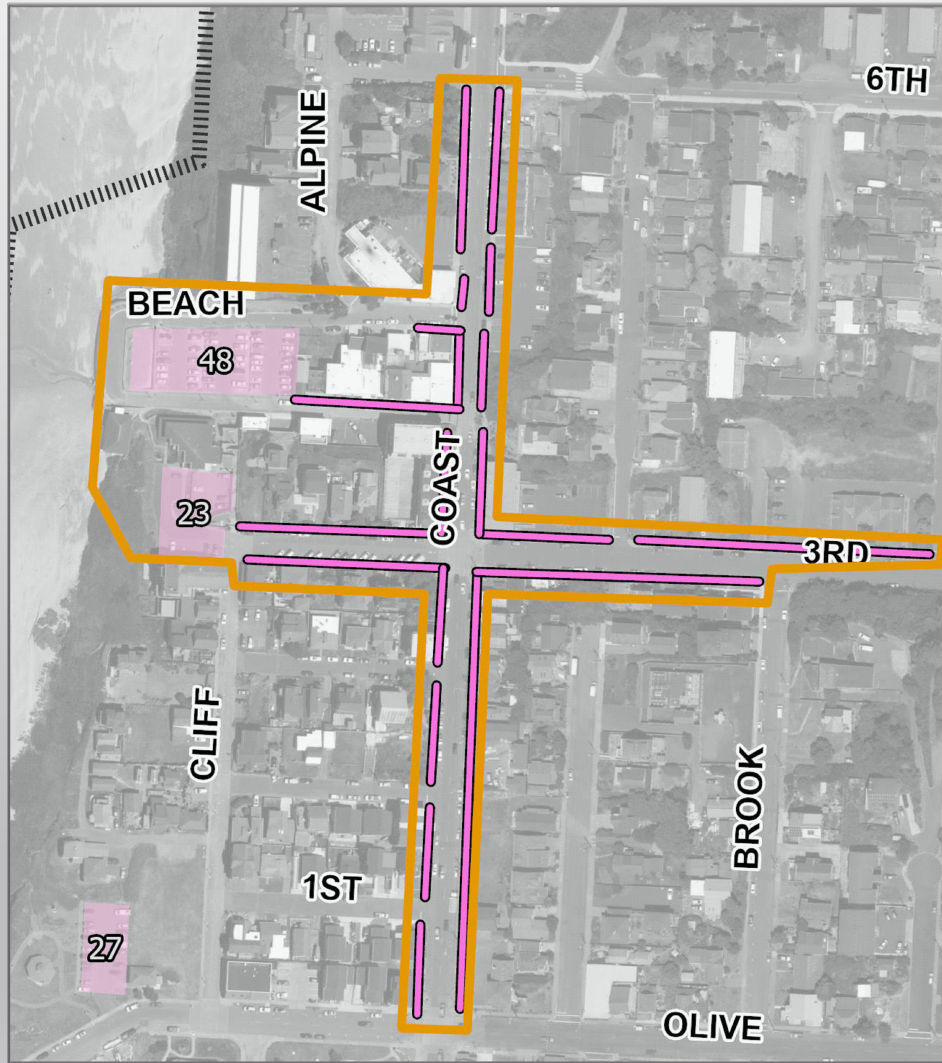
- Warnings 20%
- Paid 59% (prior to Collections)
- Court 1%





Pivot to Nye Beach

Nye Beach "Scaled Down" Parking Concept



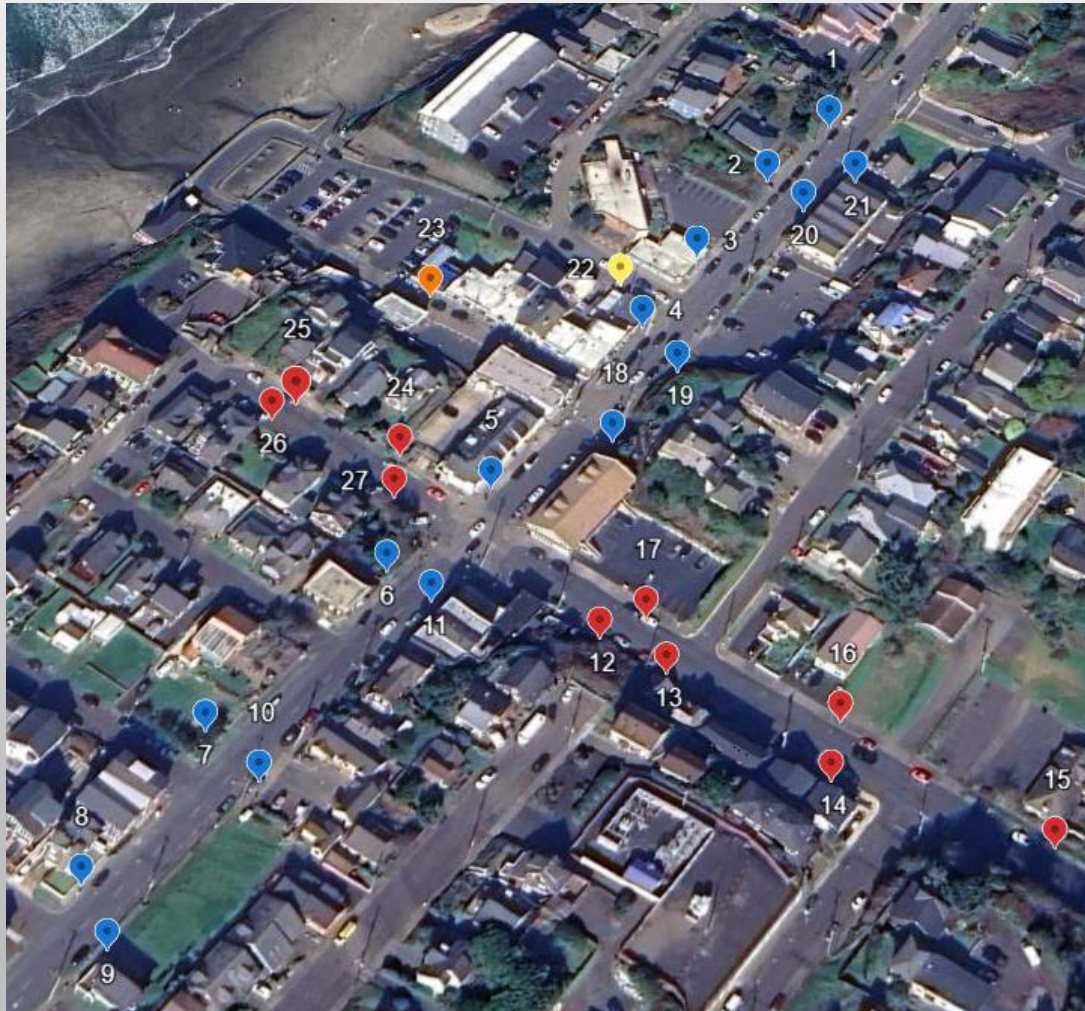
Nye Beach Turnaround Parking Lot



Visual Arts Center Lot



Permit Parking (Portion of VAC Lot, Coast, Beach and 3rd St)





Comparison Nye Beach Parking Study & Scaled Down Concept

Scaled Down Concept

- Metered: 48 Stalls
- Pay Stations: 2
- Permit/Timed: 139 Stalls
- Total Parking 187 Stalls



Contributions Equitable and Sufficient to Cover Costs?

Example: Nye Beach Parking Investment Needs



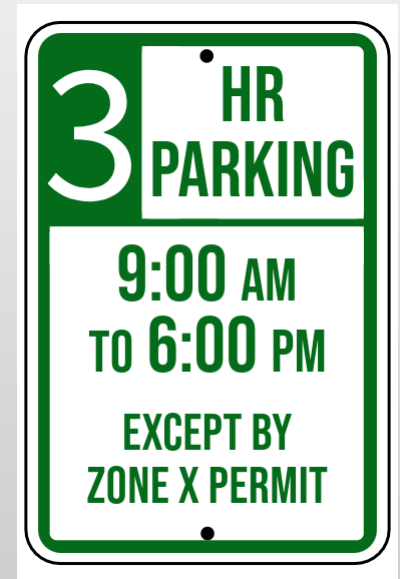
(Plan to Budget Over Two Years
Once Program is Adopted)



How Can I Get an Electronic Parking Permit?



When Could the Changes Happen?





What is the City's Plan for Managing Parking in Nye Beach?

The City's plan for managing parking is to establish paid parking and permit/timed parking areas along streets and parking lots in the commercial core of Nye Beach. The plan is based upon a parking study that the City completed with stakeholder input in 2018, and which was formally adopted in 2020.

Which Parking Areas will this apply to?

The Nye Beach Turnaround, Visual Arts Center parking lot, NW Beach Drive, and those portions of NW 3rd and NW Coast Streets currently time limited to 3 hour parking between 9-6pm. It will not apply to private lots and parking areas.

So... What is the Parking Plan?

The Nye Beach Turnaround will be metered seasonally at a set hourly rate, with an eight hour maximum stay. Persons parking in this public parking lot will be able to pay by phone using a "text to pay" option or they can use one of the pay stations that the City will be installing. Pay stations include coin, credit card, and coupon code functionality. The Turnaround is the only location in Nye Beach that is to be metered.

Electronic permits will be offered to individuals that live or work in areas where 3 hour parking limitations apply, allowing them to park for longer periods of time. The permits will only be effective in Nye Beach. Each resident in the area with a valid driver's license will be eligible to obtain a discounted annual electronic permit. A limited number of electronic permits will be made available for persons that own businesses or work in the area. Daily lodging permits will be available to guests staying at area hotels or short-term rentals. Permits will be available to purchase online through the City of Newport website. A map attached to this FAQ shows the location and pricing of the paid and permit parking areas.

Why Install "Pay to Park" Pay Stations and Charge for Permits?

The purpose of the parking pay stations and electronic permits is to increase vehicle turnover in high demand areas so that more parking is available to Nye Beach users. This will reduce congestion and improve public safety.

When will the Changes go into Effect?

The new paid parking and permit/timed parking areas will go into effect on or after June 1, 2025. Electronic permits will be available for purchase at least 30 days in advance of the effective date.

Will the Parking Limitations Apply to Disabled Individuals?

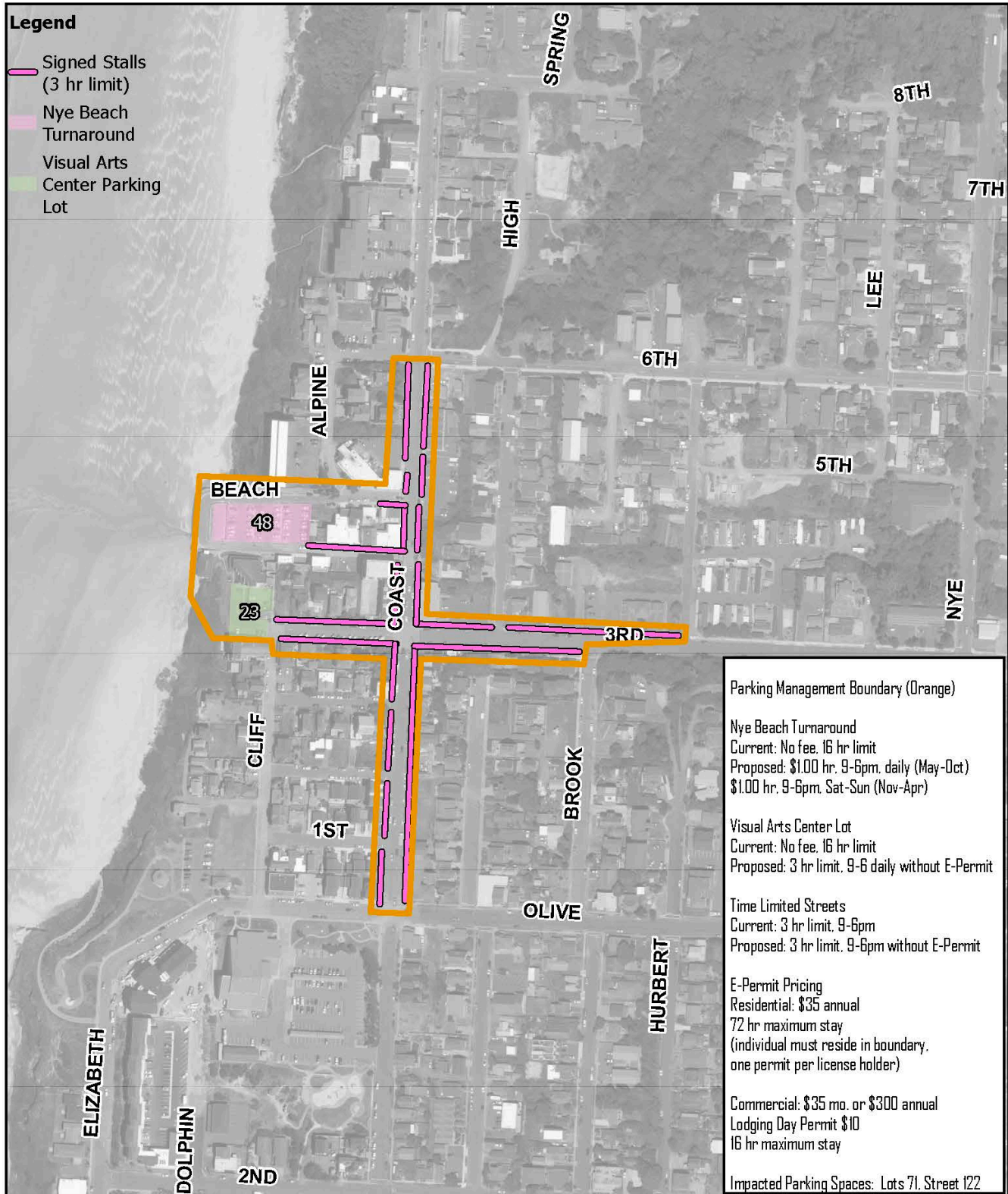
Vehicles with a state-issued disabled person registration or "wheelchair user" plate, placard, permit or decal will not be subject to posted time limits or payment requirements irrespective of whether or not they are parked in an ADA space.

How will this Impact Parking Enforcement?

The City will provide a break-in period of at least 30-days to help educate users about the new rules. They will only be issuing warnings during that time period. The City's parking enforcement officer who will be using License Plate Recognition (LPR) technology to efficiently identify vehicles parked in violation of the City's parking rules.

Who do I Contact to Learn More about Upcoming Changes?

For additional information, you can contact the City of Newport Community Development Department at 541-574-0629 or publiccomment@newportoregon.gov. You can also attend Parking Advisory Committee meetings, which are typically held on the third Wednesday of the month at Newport City Hall.



NEWPORT City of Newport
 Community Development Department
 169 SW Coast Highway
 Newport, OR 97365
 Phone: 1.541.574.0629
 Fax: 1.541.574.0644

Draft Nye Beach Parking Management Plan Concept v3

Terrain Image Taken July 2024
 4-inch, 4-band Digital Orthophotos
 GeoTerra, Inc. Eugene, OR

0 150 300 600 Feet



This map is for informational use only and has not been prepared for, nor is it suitable for legal, engineering, or surveying purposes. It includes data from multiple sources. The City of Newport assumes no responsibility for its compilation or use and users of this information are cautioned to verify all information with the City of Newport Community Development Department.

Sherri Marineau

From: Robert Emond
Sent: Tuesday, June 17, 2025 6:29 PM
To: Derrick Tokos
Cc: Sherri Marineau
Subject: Re: Possible item for parking comittiee

Yes that'd be great thanks. Robert

Get [Outlook for iOS](#)

From: Derrick Tokos <D.Tokos@NewportOregon.gov>
Sent: Tuesday, June 17, 2025 4:49:43 PM
To: Robert Emond <r.emond@newportoregon.gov>
Cc: Sherri Marineau <S.Marineau@NewportOregon.gov>
Subject: RE: Possible item for parking comittiee

Hi Robert... we can add this to the July parking advisory committee meeting agenda. Will that work?

Derrick

From: Robert Emond <r.emond@newportoregon.gov>
Sent: Tuesday, June 17, 2025 2:12 PM
To: Derrick Tokos <D.Tokos@NewportOregon.gov>
Subject: Possible item for parking comittiee

Derrick,

The public arts committee is interested in exploring the idea of putting sculptures, like there are at the PAC, in the bay front. They are thinking of one spot in one of the lots, abbey st. lot was mentioned, as well as at Fogarty. They were interested in feedback from the parking committee. Is this appropriate for the parking committee to look sat and if so, is this something that can be added to the agenda?

One other thing, they were asking about other possible locations that are city owned directly in that area. I recall discussions about lack of space down there do you have any information I could take back to them about feasibility of putting something like that in? Thanks, Robert

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Hands Across the Sand at Lincoln City

JEREMY C. RUARK
Lincoln County Leader

For approximately one hour Saturday, July 5, dozens of people gathered along the beaches in Lincoln City, not to watch the ocean or to

seek agates, but to simply hold hands.

From 11:30 a.m. to 12:30 p.m. Indivisible Lincoln City and JS Conbaum hoses the Hands Across the Sand (HAS) event along the 7-mile stretch of beach

that runs the length of Lincoln City.

"At noon, our goal is to form an unbroken chain across the sand for 15 minutes to express our commitment to the protection of our marine ecosystem, in the face of

current anti-environmental policies," organizer Shar Walbaum said. "HAS gives neighbors and strangers an opportunity to stand together out of shared love for our

See **HANDS**, page A8

has declared a drought emergency due to exceptionally low streamflows and dry conditions.

Streamflows on the Siletz, Alsea, and Yachats Rivers have been at or below historic lows, with the Siletz River reporting just 22% of its average flow by mid-June, a record low for that time of year. The Alsea River was at 40% of its average

Lincoln County.

"The drought conditions are putting stress on our drinking water supplies, as well as the fish and wildlife that depend on water in our streams," a release from Lincoln County states.

Because the county can't depend on snow-pack water runoff, it depends on Oregon's

See **DROUGHT**, page A6

NPD to begin immobilization of vehicles with outstanding parking tickets

STAFF REPORT
Lincoln County Leader

Beginning August 1, 2025, the Newport Police Department (NPD) will immobilize vehicles that have long overdue or multiple unpaid parking citations, in accordance with the Newport Municipal Code (chapter 6.50.015).

Immobilization

includes the use of a parking boot on the vehicle, one of the final steps in the process of collecting unpaid parking fines, used after other efforts have been exhausted, according to a release from NPD.

Individuals with outstanding parking citations

are encouraged to pay all fines and fees before the immobilization program begins. Fines may be paid in person at Newport City Hall, located at 169 SW Coast Highway, or online at NewportOregon.gov.

See **BOOT**, page A8



Immobilizing a vehicle includes use of a mechanical tool called a boot that locks the vehicle in place. (Courtesy from the City of Portland)



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annually from July through June, the Lincoln County Veteran Services officers conducted outreach at community events and numerous meetings with veterans and their family members, providing essential guidance on a wide range of subjects. Barnes said the efforts resulted in the successful submission of claims for service-related disability compensation, healthcare enrollments, burial allowances, and survivors' benefits for eligible widows and widowers of deceased veterans.

"We're incredibly proud of the impact our office has had on the lives of Lincoln County veterans and their families," Barnes said. "These funds provide crucial

ing the comprehensive financial relief provided to the community.

"With a general budget allocation of \$222,771, the office's ability to deliver over \$5.6 million back into the local economy demonstrates remarkable efficiency and effectiveness," Barnes said. "Lincoln County Veteran Services remains committed to serving those who have honorably served our country."

Veterans and their family members who may be unsure of their eligible benefits or have questions regarding military service are encouraged to contact the office at 541-265-0570 to schedule a confidential appointment.

LCSD organizational efficiency and strengthening district-wide support.

With the retirement of Assistant Superintendent Susan Van Liew, LCSD is streamlining central office responsibilities by naming Dr. Tiana DeVries as Deputy Superintendent of Human Resources and Aaron Belloni as Deputy Superintendent of Student Services, according to a district release.

the release states. "In addition to new duties, some responsibilities previously held by Van Liew — such as program supervision of Safety, Oregon Department of Education's Integrated Guidance, and roles like Civil Rights Coordinator — have been reassigned to other district administrators."

Two school-level leadership changes are also

and departments are well-supported," LCSD Superintendent Dr. Majalisse Tolan said.

LCSD serves students from kindergarten through 12th grade in 11 regular schools, one online school for students in seventh and eighth grade, and three public charter schools. The district operates with approximately 600 employees.

BOOT

From Page A1

WHAT WILL HAPPEN

According to the municipal code, an enforcement officer may place a boot on the vehicle without prior notice to the registered owner if the vehicle has one unpaid parking citation that is outstanding for more than 45

days, or four or more unpaid parking violations are registered to the vehicle. If a vehicle is immobilized or impounded, the vehicle will be released to the registered owner once all outstanding fines and fees are fully paid.

The City of Newport sends two notices to those with an unpaid citation. If the city does not receive a response

following the second notice, the citation is then sent to collections.

BY THE NUMBERS

Currently, 377 parking citations are overdue to the City of Newport.

In the past year, 43% of citations were paid prior to any sent notices or collection efforts, 19% of citations were

sent notices, 7.5% paid outstanding parking fines after notices were sent, 7% were sent to collections, and less than 1% paid outstanding fines after being sent to collections.

"Newport community members and visitors are encouraged to comply with the restrictions placed on parking, which allow equal access to all parking areas," the release

states. "For individuals that receive a parking citation, they are encouraged to pay the fine before collections efforts begin to avoid extra penalties and fees."

To view the Newport Municipal Code, visit <https://www.newportoregon.gov/dept/adm/documents/NewportMunicipalCode.pdf>

SAND

From Page A1

beach, our ocean. Whatever our differences, we agree that it is worth protecting. This beautiful place isn't just loved by residents of Lincoln City. Visitors come from all over Pacific Northwest — the world

— because they value this natural place.

According to Walbaum, the event drew over 200 people holding hands at six Lincoln City beach locations.

41 at D River
19 at NW 15th Street
24 at NW 26th Street
30 at NW 34th Street
10 at NW 49th Street

90 at Roads End

"Climate change is heating up our oceans, destroying marine wildlife and hurting us, too. Federal policies that were crafted to slow climate change are under assault by the current administration," Walbaum said. "This administration is putting the marine ecosystem at risk for increased

pollution. A five-year plan to increase offshore drilling for oil and gas will affect our shores, as well as the arctic. Drilling destroys the environment of the ocean bed, affecting everything that lives there. Exploration involves blasting and that damages the hearing of marine mammals that depend on sound to communicate."

Walbaum added that local, state and federal leaders need to strengthen current legislation aimed at slowing the pace of climate change, reduce the use of plastic waste pollution, and strengthening protections for public land where possible.

For more information, visit Hands Across the Sand: hasor2025@gmail.com

Sherri Marineau

From: Derrick Tokos
Sent: Thursday, July 31, 2025 8:57 AM
To: Sherri Marineau
Subject: FW: Day Use Fees for South Beach SP Day Use and South Jetty

Lets include this in the upcoming parking committee packet.

Derrick

From: Nina Vetter <N.Vetter@NewportOregon.gov>
Sent: Thursday, July 31, 2025 8:30 AM
To: Department Heads <DepartmentHeads@NewportOregon.gov>
Subject: FW: Day Use Fees for South Beach SP Day Use and South Jetty

FYI

From: FOWLER Brian * OPRD <Brian.FOWLER@oprd.oregon.gov>
Sent: Thursday, July 31, 2025 7:33 AM
To: Nina Vetter <N.Vetter@NewportOregon.gov>
Subject: Day Use Fees for South Beach SP Day Use and South Jetty

Good morning Nina,
My name is Brian Fowler and I am the Park Manager for the South Beach Management Unit. Below is a message regarding Day Use fees and rate increases that will impact community members here in Newport. Please let me know if you have any questions, and please feel free to pass this on.
Thanks.

From: GAUTHIER Katie * OPRD <Katie.GAUTHIER@oprd.oregon.gov>
Sent: Tuesday, July 29, 2025 12:10 PM
Subject: Oregon State Parks: Upcoming Changes



In recent weeks, you may have seen news reports about efforts to reimagine Oregon State Parks to address a budget gap within Oregon Parks and Recreation Department. This gap is due to increasing costs, the impact of higher visitation and changes in lottery funds.

As most of you know State Parks' funding comes from three main sources. A little less than half comes from constitutionally dedicated lottery funds, about 15% comes from the agency's share of recreational vehicle license plate fees and roughly 35% comes from park fees from visitors. Oregon State Park operations are not funded by general fund taxes.

Given these challenges, OPRD is taking proactive steps to close our budget gap and commit to diversifying our funding sources for a more resilient future for our park system. We're pursuing a three-part strategy: first, we're taking a hard look internally by evaluating our procurement/contracting, budgeting practices, and operational efficiencies; second, we're reimagining the role creative partnerships can play in improving visitor experiences and advancing park stewardship; and third, we're making targeted fee adjustments to help stabilize revenue. Each of these efforts is grounded in a commitment to long-term resilience, public service, and transparency.

We will continue to share more about the first two strategies in the coming months. However, we want to provide you information about fee increases coming to parks in your community **beginning October 1, 2025**. We plan to release information about these increases to the public later this week.

This fall, OPRD will no longer waive parking fees at about twenty new parks. The daily parking rate is currently \$10 for Oregonians and \$12 for out-of-state visitors. Individuals who are camping in a state park, have an annual state parks parking permit or bike or walk to a park are not required to pay an additional fee. The list of parks that will now charge a parking fee is listed below.

Additionally, for stays **beginning May 1, 2026**, the department will increase overnight rates seasonally at our busiest parks and increase cabin and yurt prices year-round. More information about which parks, and the types of rate increases are listed below. Our overnight camping rate ranges are available in [administrative rule](#).

We understand these necessary changes are new and bold. Folks may reach out with some frustration. We want to ensure that you are informed and prepared in case you hear from your constituents.

The bottom line is State Parks' costs are continuing to rise, and our revenue is not keeping pace. If we do nothing, we risk destabilizing the park system we've all worked hard to protect. The rising costs of everything coupled with a 13% reduction in lottery funds for operations means this budget gap will only grow. These fee changes are one part of a broader effort to maintain service levels, strengthen our system, and create new opportunities for visitors, partners, and staff alike.

We are acting now to ensure the parks we love can thrive long into the future. Please let us know if there is any additional information you need.

Sincerely,

Katie Gauthier
Government Relations Manager

Oregon Parks and Recreation Department Fee Increase Table

State Park	Day Use Parking Fee Beginning October 1, 2025	Seasonal Rate Adjustments: RV and Tent sites for stays after May 1, 2026	Cabin and Yurts Year- Round, for stays after May 1, 2026
Ainsworth State Park		x	
Alfred A. Loeb State Park		x	x
Beachside State Recreation Site		x	x
Beverly Beach State Park	x	x	x
Bullards Beach State Park	x	x	x
Cape Blanco State Park		x	x
Cape Lookout State Park		x	x
Carl G. Washburne Memorial State Park		x	x
Champoeg State Heritage Area		x	x
Cline Falls Scenic Viewpoint	x		
Cottonwood Canyon State Park			x
Cove Palisades State Park		x	
Crown Point State Scenic Corridor	x		
Deschutes River State Recreation Area		x	
Detroit Lake State Recreation Area		x	x
Devils Lake State Recreation Area		x	x
Dexter State Recreation Area	x		
Emigrant Springs State Heritage Area			x
Farewell Bend State Recreation Area	x		x
Fort Stevens State Park	x	x	x
Harris Beach State Park	x	x	x
Hat Rock State Park	x		
Humbug Mountain State Park		x	
Jessie M. Honeyman Memorial State Park		x	x
L.L. Stub Stewart Memorial State Park		x	x
Lake Owyhee State Park			x
LaPine State Park	x	x	x
Lewis and Clark State Recreation Site	x		
Lowell State Recreation Site	x		
Milo McIver State Park		x	
Nehalem Bay State Park		x	x
Oswald West State Park	x		
Prineville Reservoir State Park	x		x
Silver Falls State Park		x	x
South Beach State Park	x	x	x
Starvation Creek State Park	x		

Sunset Bay State Park	x	x	x
Tolovana Beach State Recreation Site	x		
Tryon Creek State Natural Area	x		
Tumalo State Park		x	x
Umpqua Lighthouse State Park		x	x
Unity Lake State Recreation Site			x
Valley of the Rogue State Park	x	x	x
Wallowa Lake State Park		x	x
William M. Tugman State Park		x	x



Katie Gauthier

Government Relations & Interim Communications
Manager

Oregon Parks and Recreation Department

725 Summer St. NE, Suite C, Salem OR 97301
Cell: 503-510-9678 she/her



Brian Fowler | South Beach MU | Park Manager

Oregon Parks and Recreation Department
5580 S. Coast Hwy, Newport Oregon
Office-541-867-7451
Cell-541-270-2570